



Public Works works for the Public



COUNTY OF IMPERIAL

DEPARTMENT OF PUBLIC WORKS

155 S. 11th Street  
El Centro, CA  
92243

Tel: (442) 265-1818  
Fax: (442) 265-1858

Follow Us:



[www.facebook.com/ImperialCountyDPW/](https://www.facebook.com/ImperialCountyDPW/)



<https://twitter.com/CountyDpw/>

## COUNTY OF IMPERIAL PUBLIC WORKS

**Request for Proposals: Architectural Feasibility Study and Conceptual Design Services County of Imperial Public Health Department Multi-Program Central Relocation Located At 450 West Aten Rd, Imperial, CA 92251 County Project Number: SR7111HTH**


### ADDENDUM NO. 1

**February 12, 2024**

This *ADDENDUM* is hereby made part of the Contract Documents and specifications to the same extent as if originally included therein and shall be signed by the Bidder and included with the proposal.

1. **Can the County please confirm that the building in question is currently not in use by any tenant?**
  - a. Although the building has been leased to various small businesses in the past, as of today, no section of the building is currently occupied by any tenants.
2. **Can the County please confirm that exact square footage of the building in question?**
  - a. The examination of Exhibit (A) within the "Description of General Dynamics Section - Building Size," report has the building measurement as 65, 440 square feet. However, if the consultant requires more precise measurements, the requirement should be noted in the consultant proposal.
3. **Does the County have a floor plan of the building?**
  - a. All information that the county can provide will be detailed within Exhibit (B).
4. **Can the county please confirm if there has been a Phase 1 environmental report done for the property?**
  - a. No, the County does not have a Phase I environmental report completed for this property.
5. **Can the county please confirm that the RFP responses will include building evaluations by Structural, Mechanical, Electrical and Plumbing.**
  - a. The current scope of work only requires a feasibility study. Prospective consultants may choose to provide structural, mechanical, electrical, and plumbing recommendations within their proposals/cost proposals as optional tasks.
6. **Can the county please confirm that a cost estimate is to be included in the scope. What is the level of accuracy of this estimate?**
  - a. The consultant is expected to furnish a conceptual level estimate. This should encompass a thorough breakdown of anticipated expenses, providing a clear and transparent overview of the financial considerations associated with each task.
7. **Can the county please confirm that a seismic evaluation has been completed for the building?**
  - a. No, the County does not have a seismic evaluation for this building.
8. **Can the county please confirm if the building is in a flood zone?**
  - a. As per the FEMA Flood Map Service Center, ascertained through their 'Search by Address' web page, it is confirmed that the parcel in question is not situated within a floodplain.

- 9. Does the county have any info on the FAA zone limitations?**
- a. The sole information accessible from the county is documented within Exhibit D. Any supplementary information required will be incumbent upon the consultant to procure.
- 10. Can the county confirm if there is roof access to the building?**
- a. Roof access is not required at this stage of the process but will be provided to the successful bidder.
- 11. Can the county verify if there were there any schedule assumptions on the length of this first phase of the project that we are pricing? Is it a 12-week schedule for this work or is it longer?**
- a. The consultant is hereby requested to submit a meticulously itemized schedule, delineating the scope of work, corresponding timelines, and the projected duration for the successful completion of the project. This crucial task is to be undertaken as the primary responsibility of the selected firm, ensuring a comprehensive and transparent overview of the project's progression.
- 12. Can the county verify if they will be precluding consultants if they are not located within the Imperial County Limits?**
- a. No consultant shall be disqualified based on geographic location; eligibility will be determined by the consultant's adherence to the defined scope of work and alignment with the criteria stipulated within the scoring rubric (See Exhibit D in original RFP ). A consultant meeting these requirements should be duly considered eligible, irrespective of their location outside the Imperial Valley.
- 13. Can the county verify if there is there a geotechnical engineer's report available for the RFP?**
- a. Currently, all available information the county has on the project can be found in Exhibit E.
- 14. Can the county verify if there is there a hazardous material report available for the RFP?**
- a. No. The County does not have a hazardous material report available for this RFP.
- 15. Can the county provide as-builts for the building available for the RFP?**
- a. The original plans for the building in question are accessible; however, they date back to 1989 (refer to Exhibit A). As highlighted in response to Question 4, any updated plans necessitate coordination with the City of Imperial. The onus to obtain current building plans from relevant regulatory agencies rests with the successful bidder, as part of their contractual responsibilities.
- 16. Can the county confirm if an EVCS system is included in the study?**
- a. No, an EVCS system is not a requirement for this study.

  
\_\_\_\_\_  
John A. Gay, P.E.  
Director of Public Works

**Acknowledgement of Addendum No. 1**

**The general contractor is responsible for advising any and all subcontractors of this change. Each bidder must acknowledge receipt of this addendum in the noted space below and where indicated on the Bidder's Proposal Section of the Special Provisions. This Addendum must be attached to the proposal.**

License No: \_\_\_\_\_

Print or Type Company Name: \_\_\_\_\_

Print or Type Authorized Name: \_\_\_\_\_

Authorized Signature of Contractor: \_\_\_\_\_

Date Signed: \_\_\_\_\_

**EXHIBIT A Description of General Dynamics  
(See Next Page)**



Description  
450 Aten Road  
Imperial, CA 92251

**Property Type:** Industrial/office building

**Location:** Approximately 1/3 mile west of Highway 86, on the north side of Aten Road  
Approximately four miles west of Highway 111  
Approximately three miles north of Interstate 8

**Assessor's Parcel Number:** A portion of 043-500-015-00

**Legal Description:** Located within parcel map showing Blocks 34, 39, 40 & 41 & Por. Block 34 Park Add. & Blocks 97 & 112, Por. Block 98, Imperial Subdivision 1 and Por. Tracts 101 & 102, inside the property boundary of Imperial County Airport. Township 15S, Range 13/14E, SBBM.

**Thomas Guide Map Page & Grid:** Page 6499, Grid F/2 (2006 Ed., San Diego County Street Guide Including Parts of Imperial County)

**Street Frontage & Visibility:** 438' frontage on Aten Road (four-lane roadway)

**Off-Site Improvements:**

- Streets: Aten Road is a four-lane roadway
- Curbs/Gutters: Concrete Curbs and gutters front subject property
- Traffic Signals: Traffic signal at Aten Road and Highway 86, approximately 1/4 mile east of subject property

**Access:** Access to the property is from Aten Road

**Site Size (Net):** 6.39 Acres; 278,348 SF (445' X 625')

**Site Shape:** Rectangular

**Building Size:** 65,440 SF  
Allocated:  
8,125 SF Office Area  
1,269 SF Break Room  
56,046 SF Work/Assembly Area  
65,440 SF (Including paint room, bake room, restrooms, electrical, meter and dust room)

**Building Shape/Dimensions:** Rectangular (290' x 225', plus additional 190 SF)

**Building Type:** Steel frame and metal skinned exterior

**Architectural Style:** Standard metal industrial building

**On-site Parking:** Paved parking with chain link fence and security wire on three sides, block wall on fourth side; security guard booth for entry to parking area

**Site Coverage:** 23.5%; City of Imperial allows a 1 : 3 building-to-land coverage ratio in industrial areas, indicating one or more buildings totaling approximately 90,000 SF could be built on this site

**Interior Description:**

- **Walls** Office: standard wood frame with sheet rock  
Warehouse/Manufacturing: metal surface
- **Plumbing Fixtures:** One men's and one women's restroom in office area; one men's restroom with four stalls and women's restroom with four stalls in warehouse/manufacturing area
- **Electrical:** Heavy duty electrical room
- **Telephone:** Installed
- **Lighting:** Dropped acoustical ceilings with recessed lighting throughout building
- **Ceiling Height:** 18' by building plans; varies depending on area of building
- **Air Conditioning:** Air conditioned throughout
- **Fire Protection:** Overhead fire sprinkler system throughout building; fire extinguishers; Imperial County Fire Department Substation located proximate, at the Imperial County Airport
- **Attached Equipment:**
  - Two overhead parallel electric cranes in one area (half-ton capacity each)
  - Industrial paint booth (12' X 29' X 8') with exhaust system
  - Industrial "bake" room to bond and cure metal finishes
  - Miscellaneous stainless steel fixtures for specific manufacturing processes
  - Multiple industrial air compressors on west end and north side of building
  - Multiple dropped 4 3/4" X 4 3/4" metal utility poles in manufacturing area, each with four electrical outlets in base and each with 4 Hansen 5000 3/8" "Quick Disconnect" hi-pressure air lines with copper adaptors

**Truck Loading Dock:** Recessed loading dock for two semi-trailer trucks with hydraulic fixtures on west side of building

**Roll-Up Door:** A single roll-up door is located on the west side of the building

**Landscaping/Irrigation:** 80' setback on front of building is landscaped and irrigated

**Emergency Decontamination:** Emergency decontamination shower facility separate from main building on west side of property

**Zoning:**

- **General Plan** I-1, Industrial, City of Imperial
- **Zoning Map** I-1, General Industrial Zone, City of Imperial

**Adjacent Uses:**

- **North** FedEx warehouse and distribution center; Imperial County Airport
- **South** Miscellaneous heavy commercial and service uses
- **East** Clairemont Equipment Rental yard
- **West** Imperial Business Park

**Airport Noise Corridor:** Yes, within the boundaries of Imperial County Airport

**Topography:** Level at street grade

**Drainage:** Retention basin on site

**Utilities:** Public utilities, including water, sewer, electricity, telephone and natural gas are connected

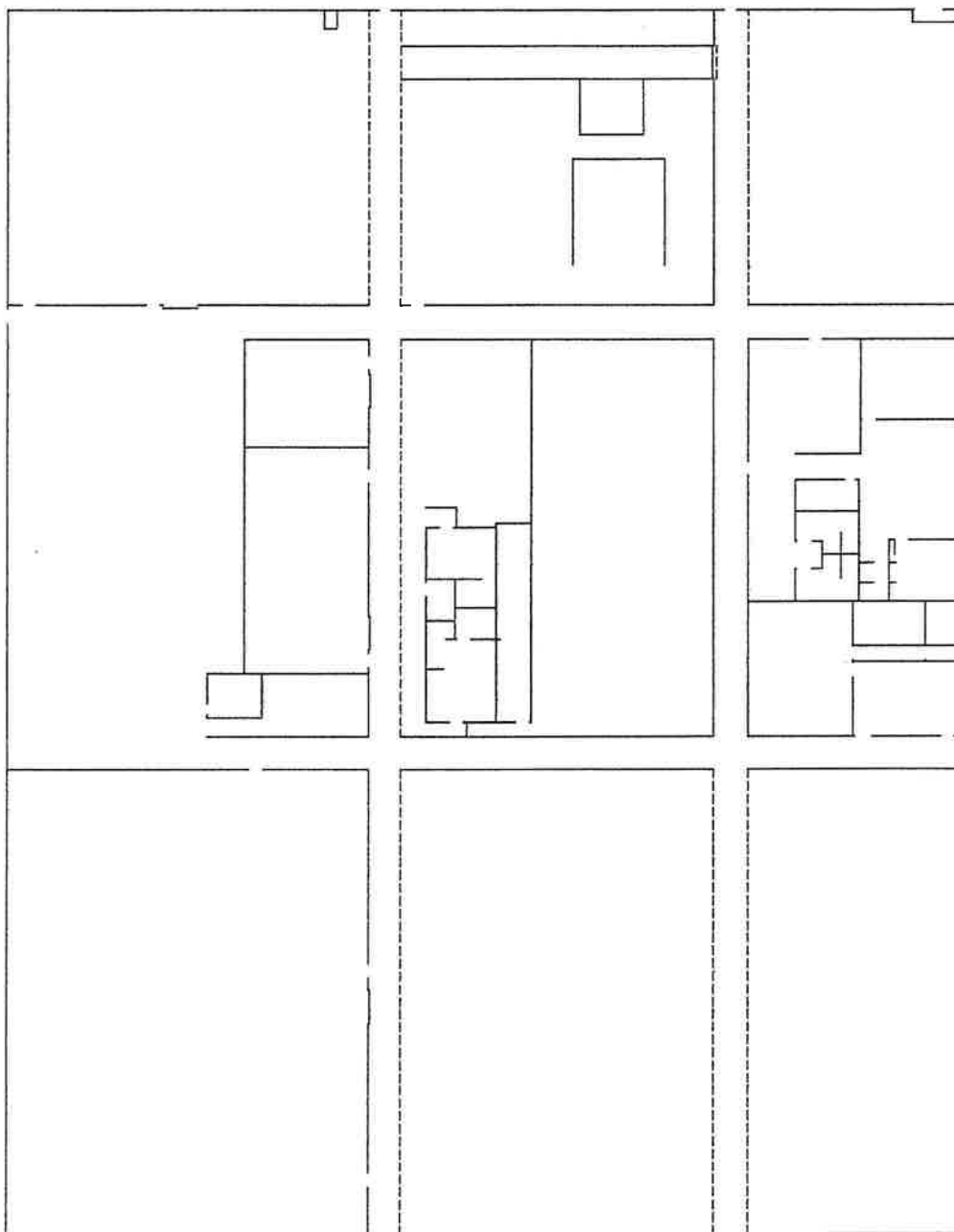
Lease Terms  
450 Aten Road  
Imperial, CA 92251

<b>Availability:</b>	Immediate
<b>Term:</b>	From 10 to 33 years - negotiable
<b>Rental Rate:</b>	\$ 0.61 SF per month - negotiable
<b>Lease Type:</b>	NNN
<b>Estimated Real Property Tax:</b>	\$42,198 annually (based on appraised value of \$4,015,000 and Possessory Interest Tax at the rate of 1.0510)

Parking Entrance

Rear  
Parking

Aten Road

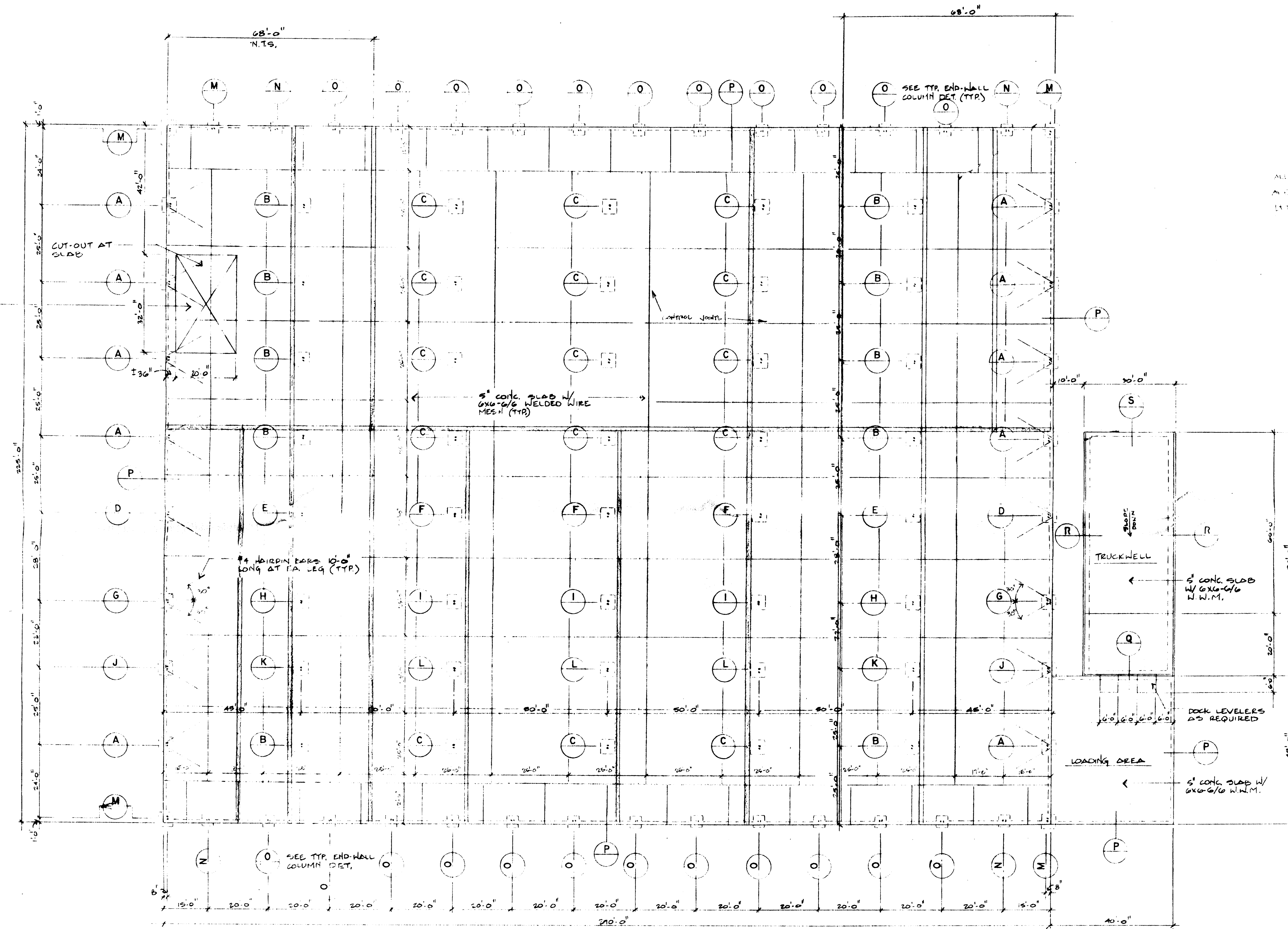


SCALE: N.T.S.

COUNTY OF IMPERIAL  
FORMER GENERAL DYNAMICS  
450 W. ATEN ROAD  
IMPERIAL, CALIFORNIA

**EXHIBIT B**  
**(See Next Page)**

SEE ENLARGED  
PLAN FOR THIS  
AREA



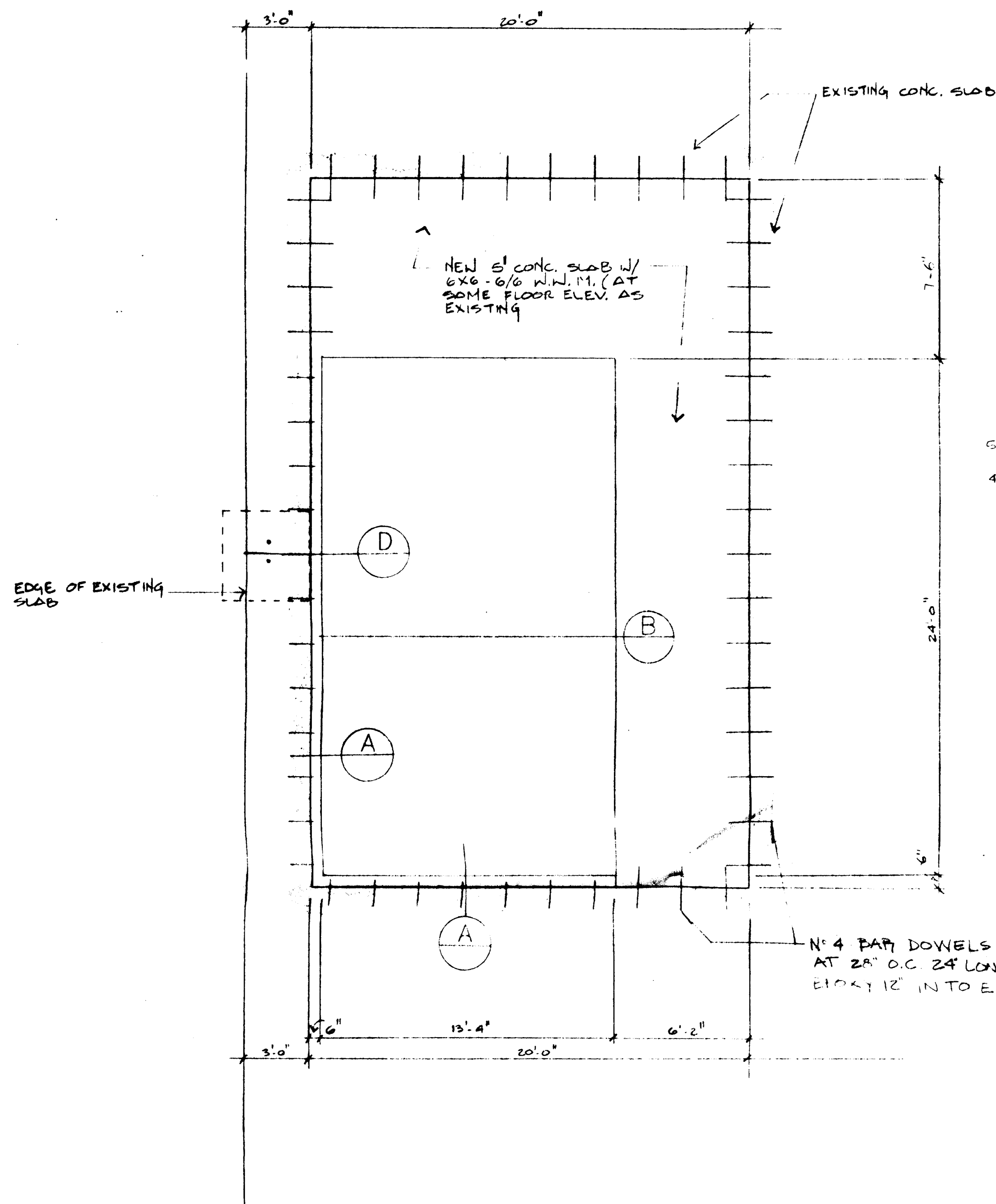
**NOTE 1:**  
ALL CONCRETE FOOTINGS SHOWN  
AS NOT TO SCALE, SEE FOOTING  
DETAIL SHEET FOR SPECIFICATIONS.

FOUNDATION PLAN  
Scale: 1/16" = 1'-0"  
North

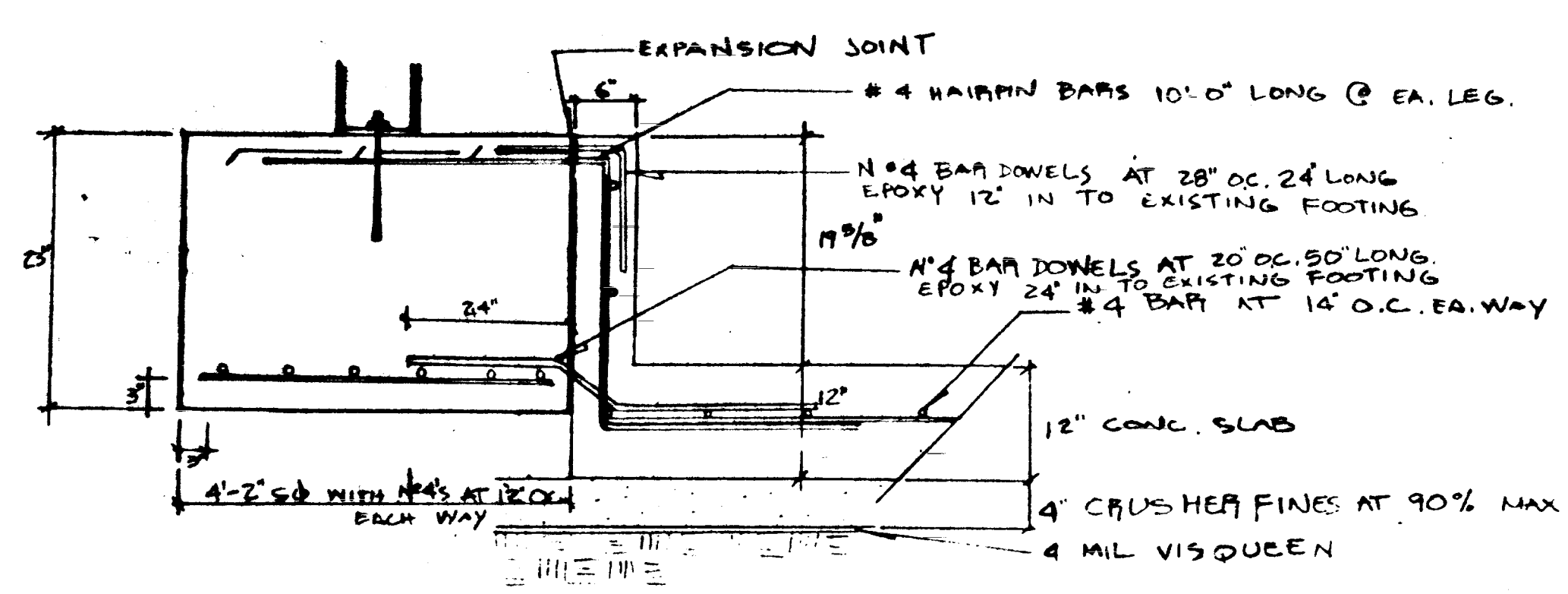


<b>DUGGINS CONSTRUCTION INC.</b> 266 CEDAR ST., EL CENTRO, CA 92243 619.352.5600			GENERAL DYNAMICS / CONVAIR DIVISION IMPERIAL, CA.	
			SCALE NOTED	APPROVED BY
DATE		REVISD 9-15-89		REVISED 9-15-89
FOUNDATION PLAN			DRAWING NUMBER	
			4 of 13	

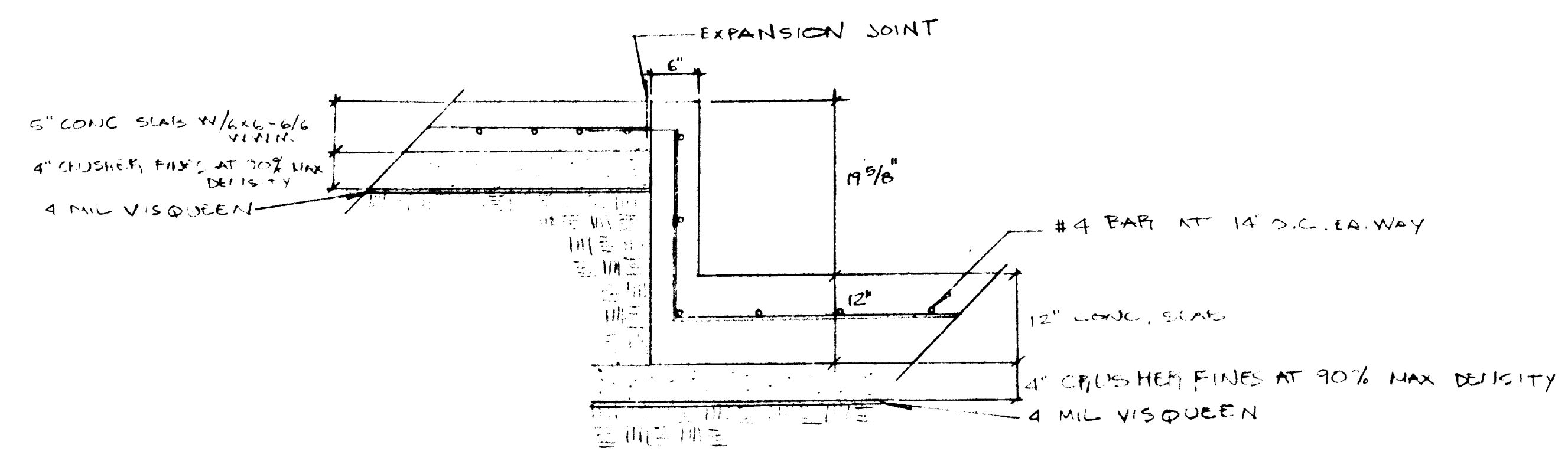




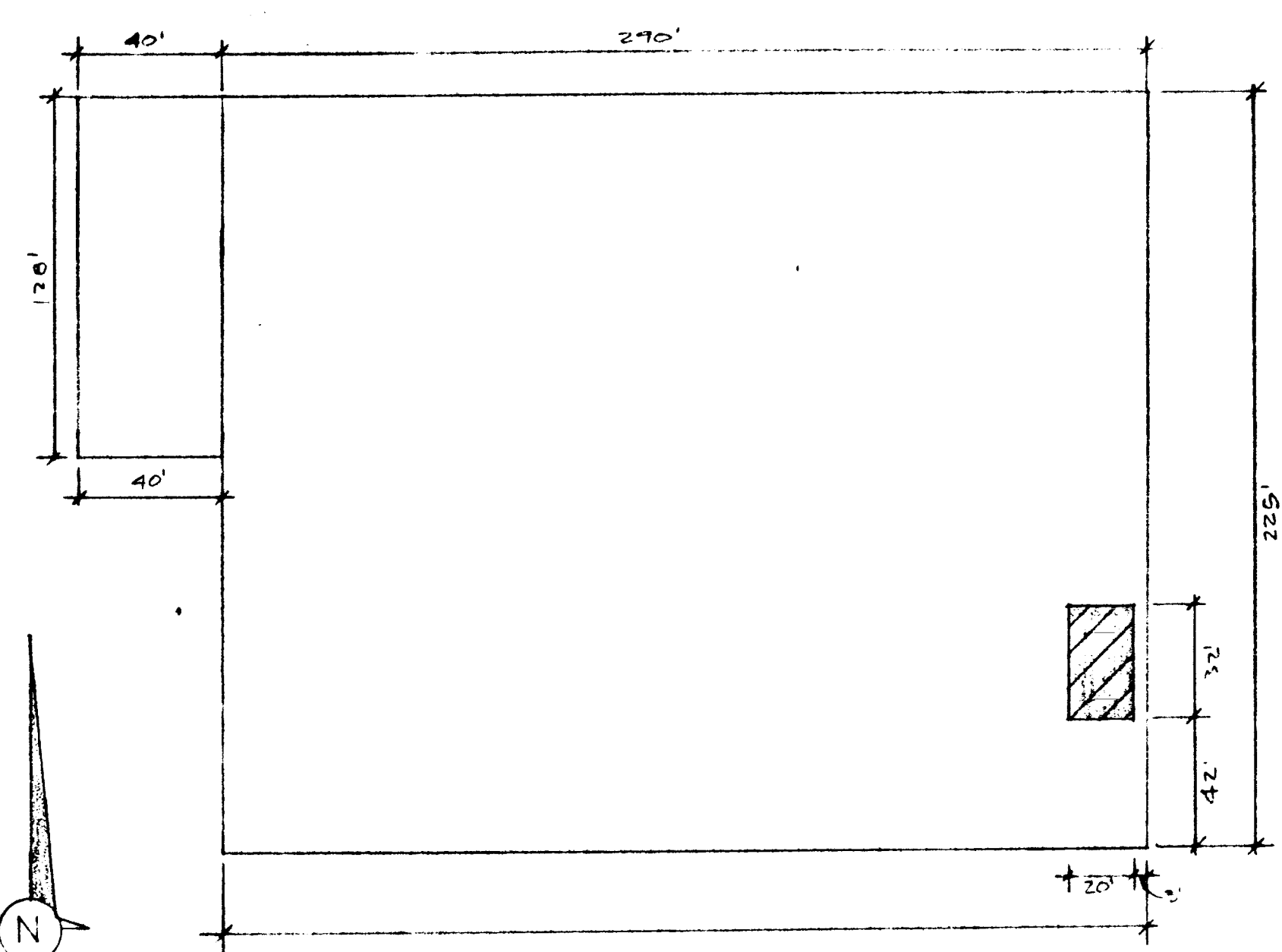
ENLARGED FOUNDATION PLAN  
1/4" = 1'-0"  
NORTH



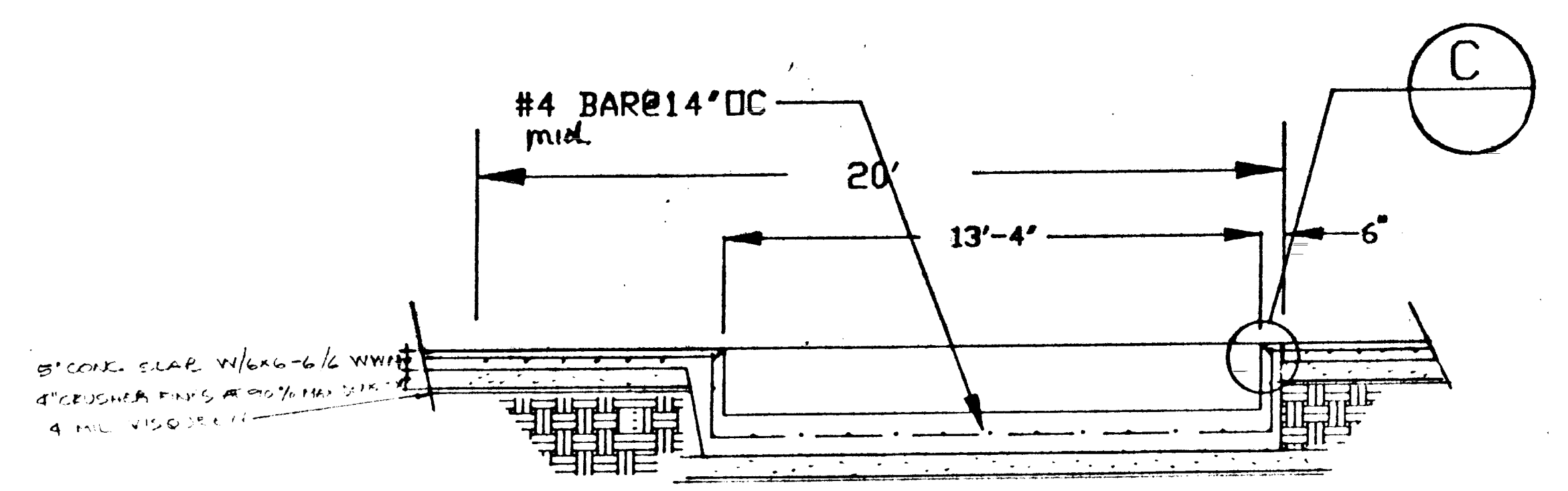
D DETAIL FOOTING & HAIRPIN



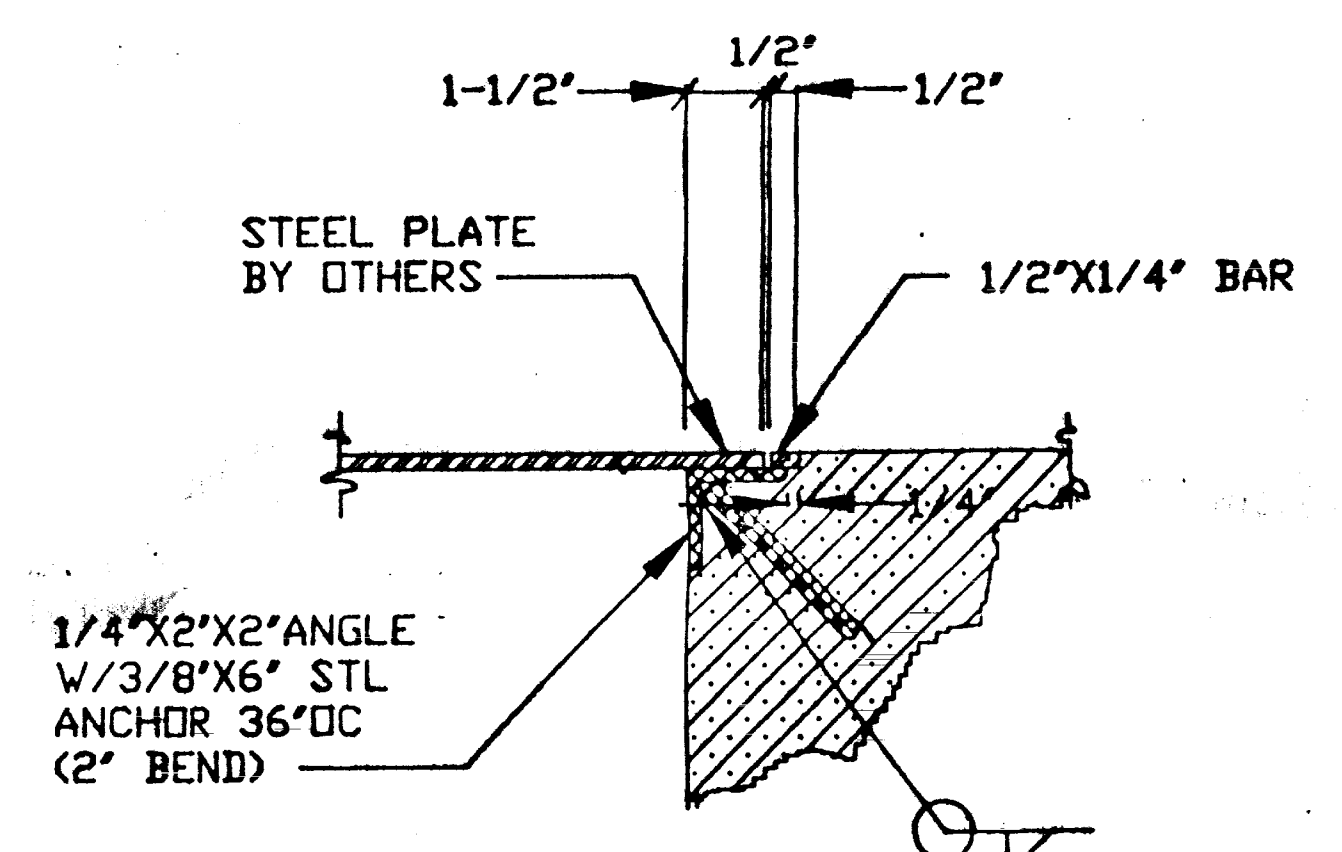
A DETAIL DOWEL WALL  
NTS



DETAIL SITE



SECTION VIEW FOUNDATION  
NTS



ANCHOR DETAIL  
NTS

NOTES

- ① CONCRETE SHALL BE MINIMUM 3000 PSI AT 28 DAY BREAK TEST
- ② TOE OF CONCRETE WALLS TO BE FLUSH WITH FLOOR OF SLAB AND HAVE LIGHT BROOM FINISH.
- ③ TROWEL FINISH OF SLAB WALLS.
- ④ ALL HOLD DOWNS SHALL BE HELD IN TRUE ALIGNMENT, ELEVATION, PLUMB WHILE POUR IS IN PROGRESS.
- ⑤ EXPANSION JOINT TO BE 3/4" ASPHALT IMPREGNATED WITH CELOTEX.
- ⑥ REINFORCING STEEL SHALL BE INTERMEDIATE GRADE BILLET STEEL ASTM DESIGNATION A615 GRADE 4.
- ⑦ ALL DIMENSIONS SHOWN UNLESS OTHERWISE NOTED.



<b>DUGGINS CONSTRUCTION INC.</b> 966 CEDAR ST., EL CENTRO, CA 92243 619-352-5600		GENERAL DYNAMICS - CONVAIR DIVISION IMPERIAL, CA.	
		SCALE: NOTED	APPROVED BY
DATE		ENLARGED FOUNDATION PLAN	
DRAWING NUMBER		4 of 18	



RECEIVED

BUILDING  
COUNTY OF IMPERIAL

INSPECTION

NOV 2 1989  
DIVISION

CALIFORNIA COUNTY  
BUILDING DIVISION

APPROVED

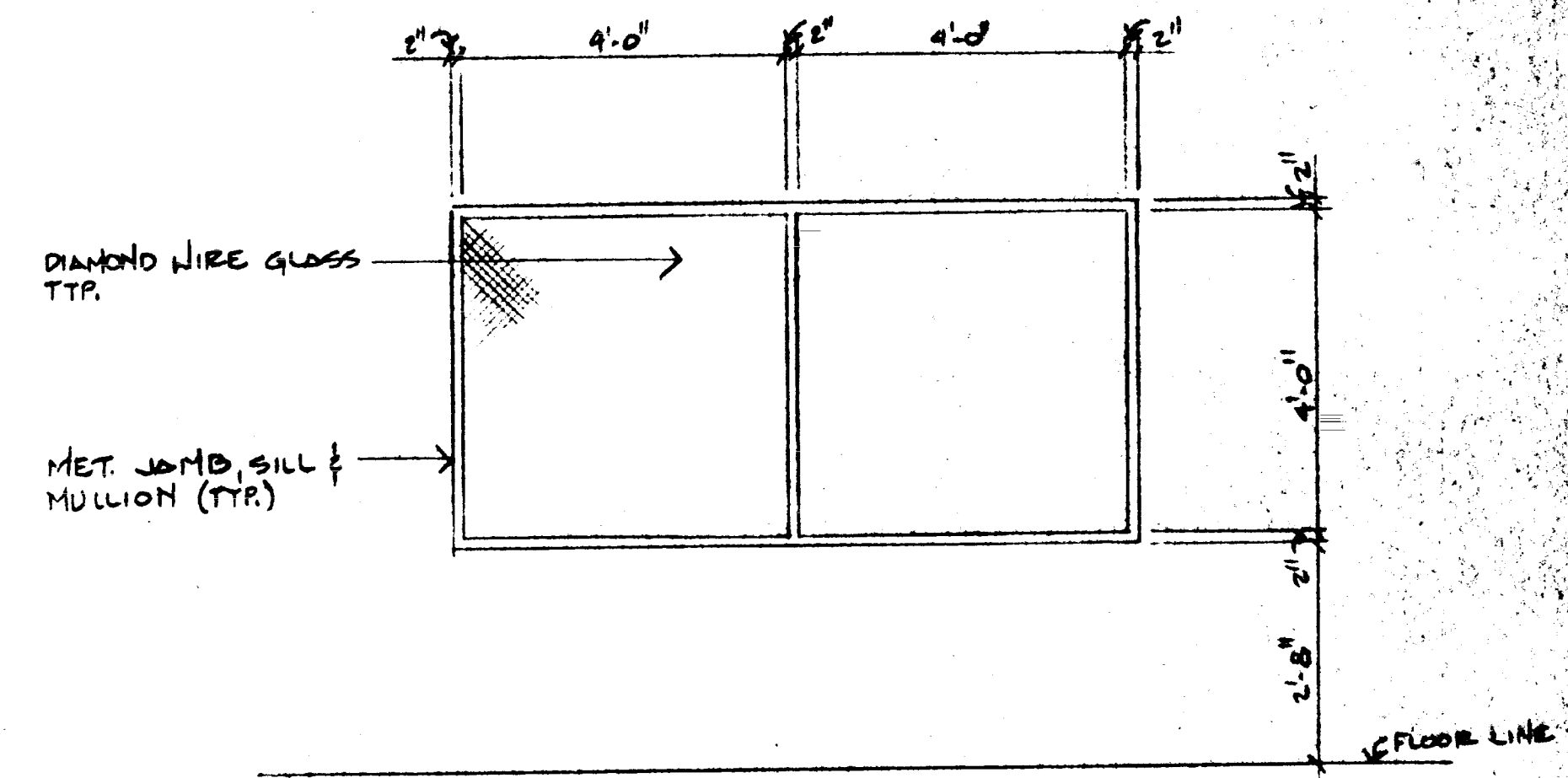
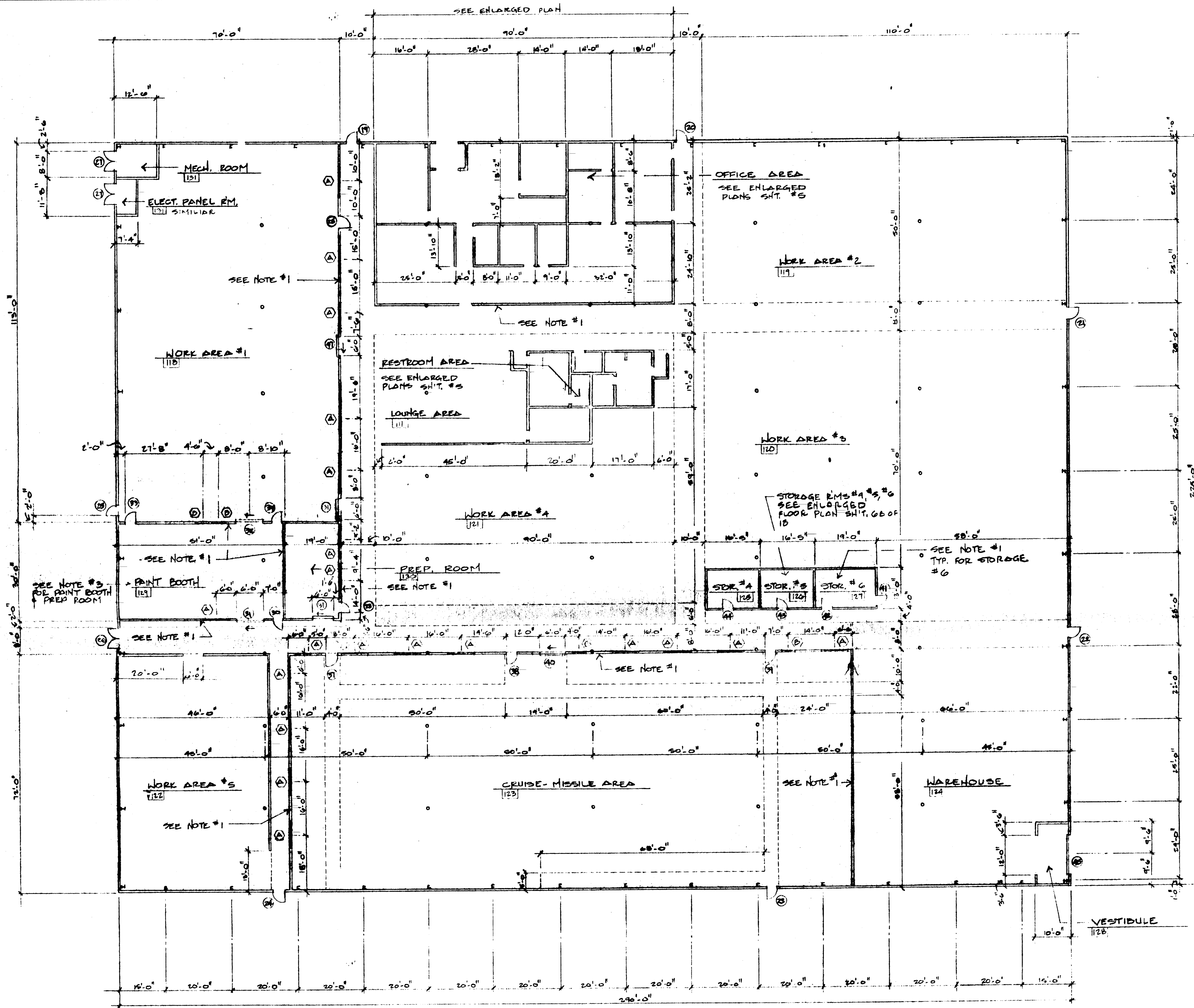
ZONE..... PERMIT NO. 29789

SETBACK: FRONT..... REAR..... SIDE.....

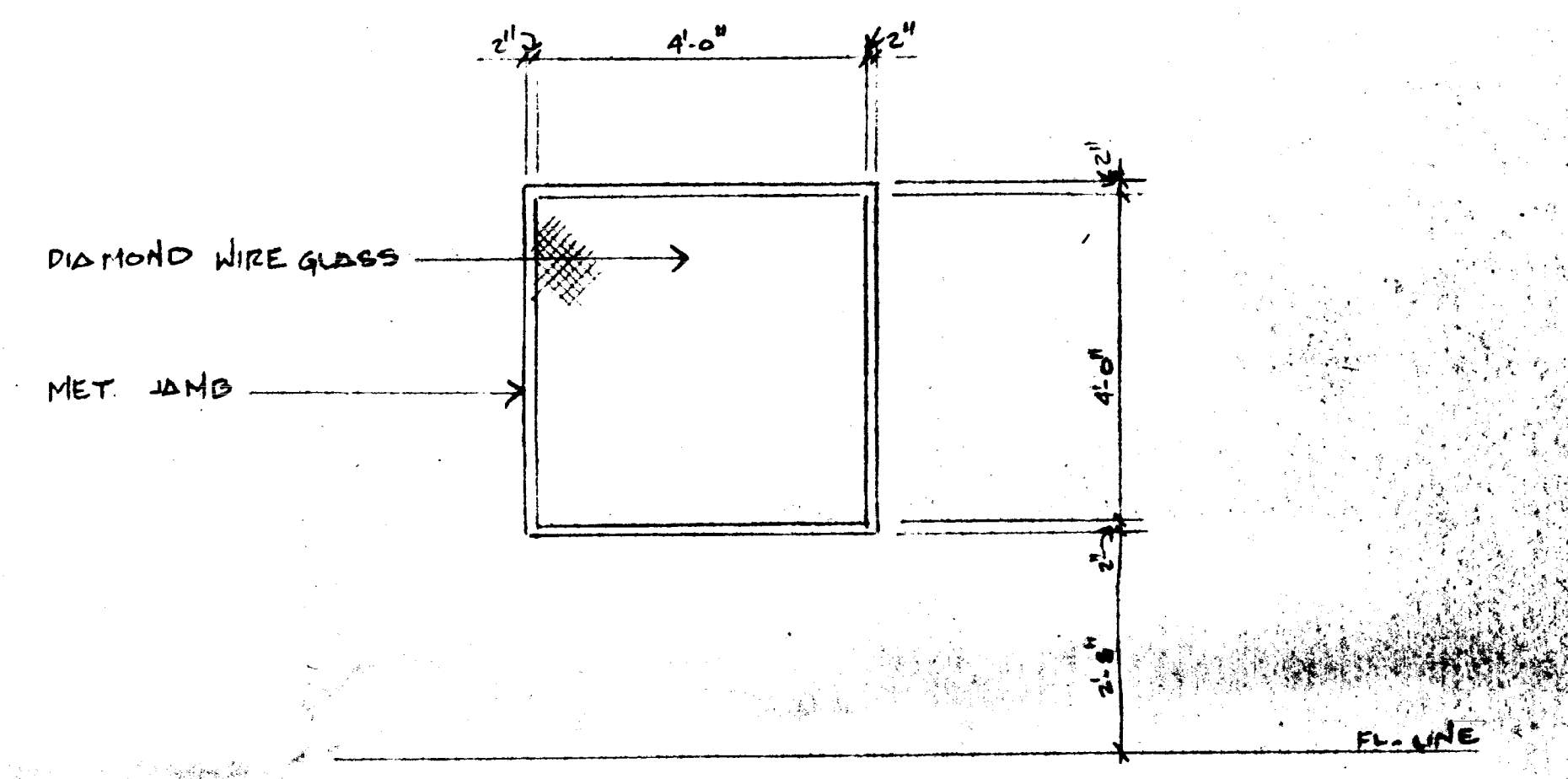
APPROVED BY: *John J. Moman* DATE: 11-16-89

THIS IS NOT A BUILDING PERMIT.

OFFICE COPY

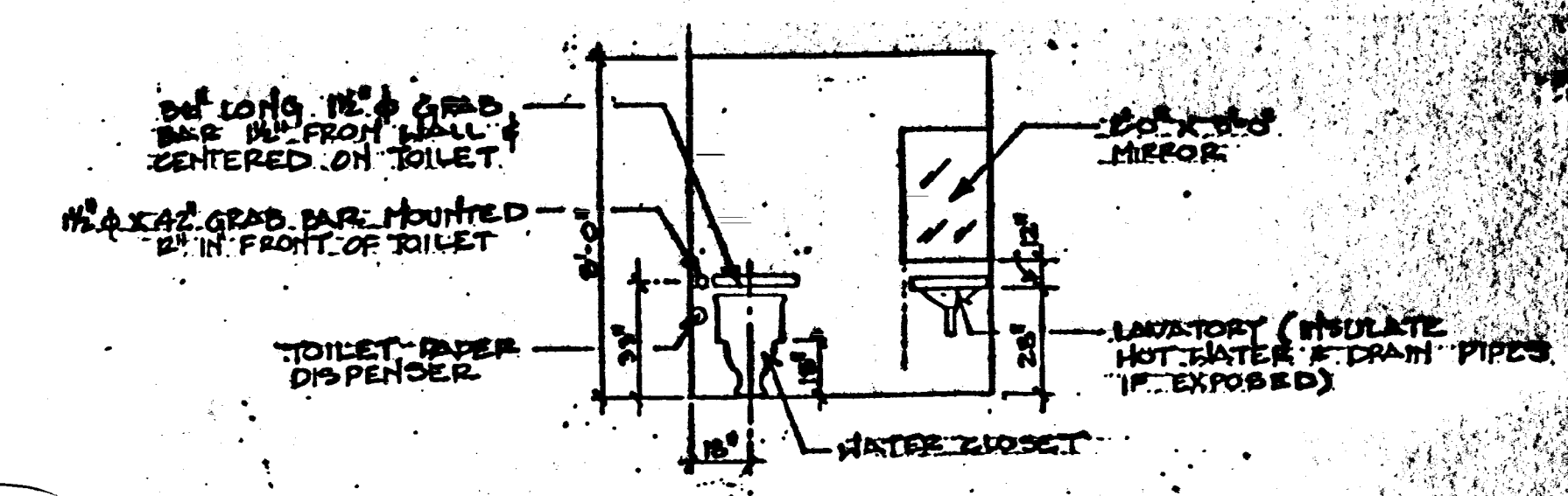


A DOUBLE WINDOW ELEVATION  
1/2" = 1'-0"



B SINGLE WINDOW ELEVATION  
1/2" = 1'-0"

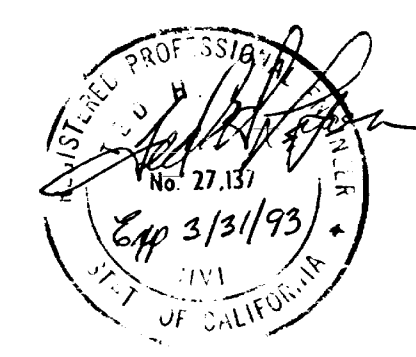
NOTE:  
ALL WINDOWS AT FIRE RATED WALLS  
SHALL COMPLY WITH 1985 U.B.C. SECTION  
4306 (i)



C TYP. TOILET ELEVATION  
SCALE: 1/4" = 1'-0"

FLOOR PLAN  
1/16" = 1'-0"

- NOTES:
- 2x4 FRAMED WALL @ 24" O.C. W/ S/B TYPE 1/4" GTP. B.D. AT EA. SIDE AS REQUIRED FOR 1-HR. FIRE RATING
  - BUILDING TO BE FIRE SPRINKLED AS REQUIRED BY CODE
  - ROOMS FOR POINT BOOTH TO BE PROVIDED. POINT BOOTH TO BE INSTALLED AT LATER DATE N.I.C.

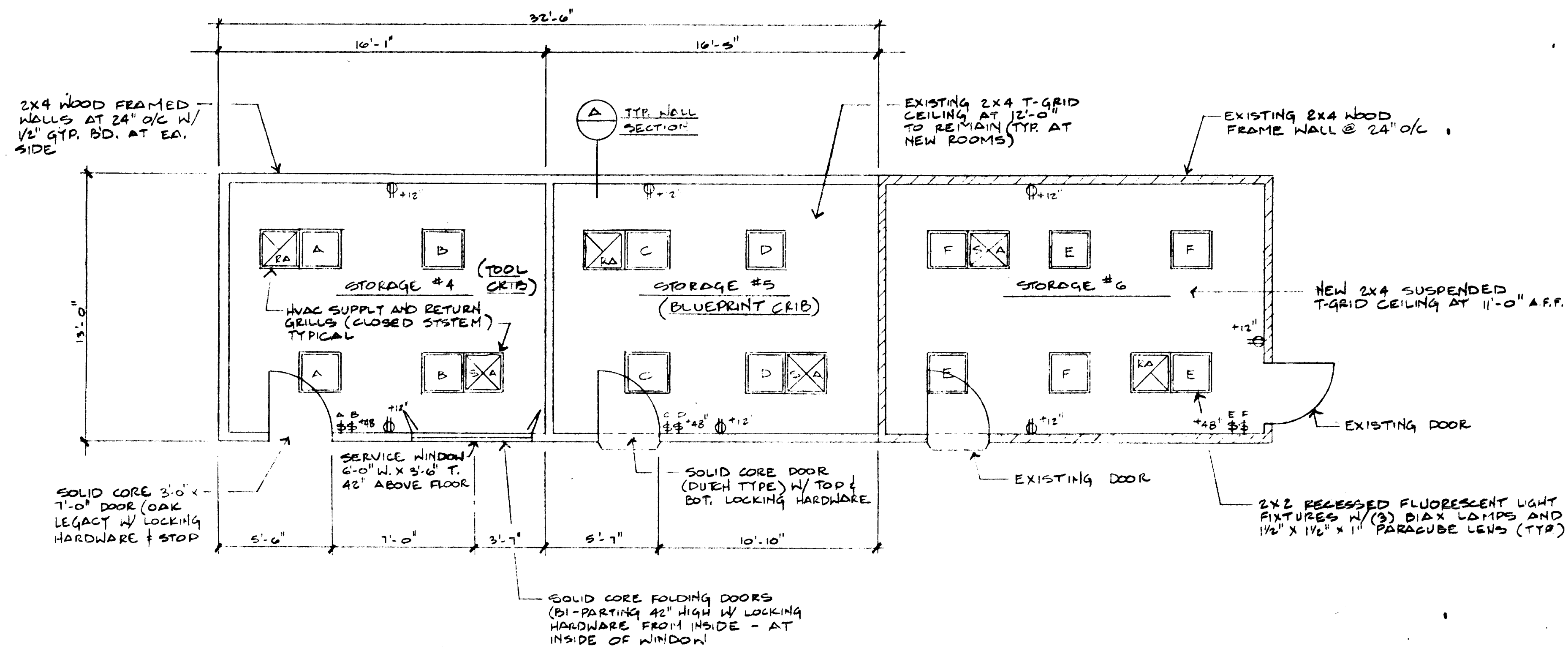


**DUGGINS CONSTRUCTION INC.**  
966 CEDAR ST., EL CENTRO, CA 92243  
619-352-5600

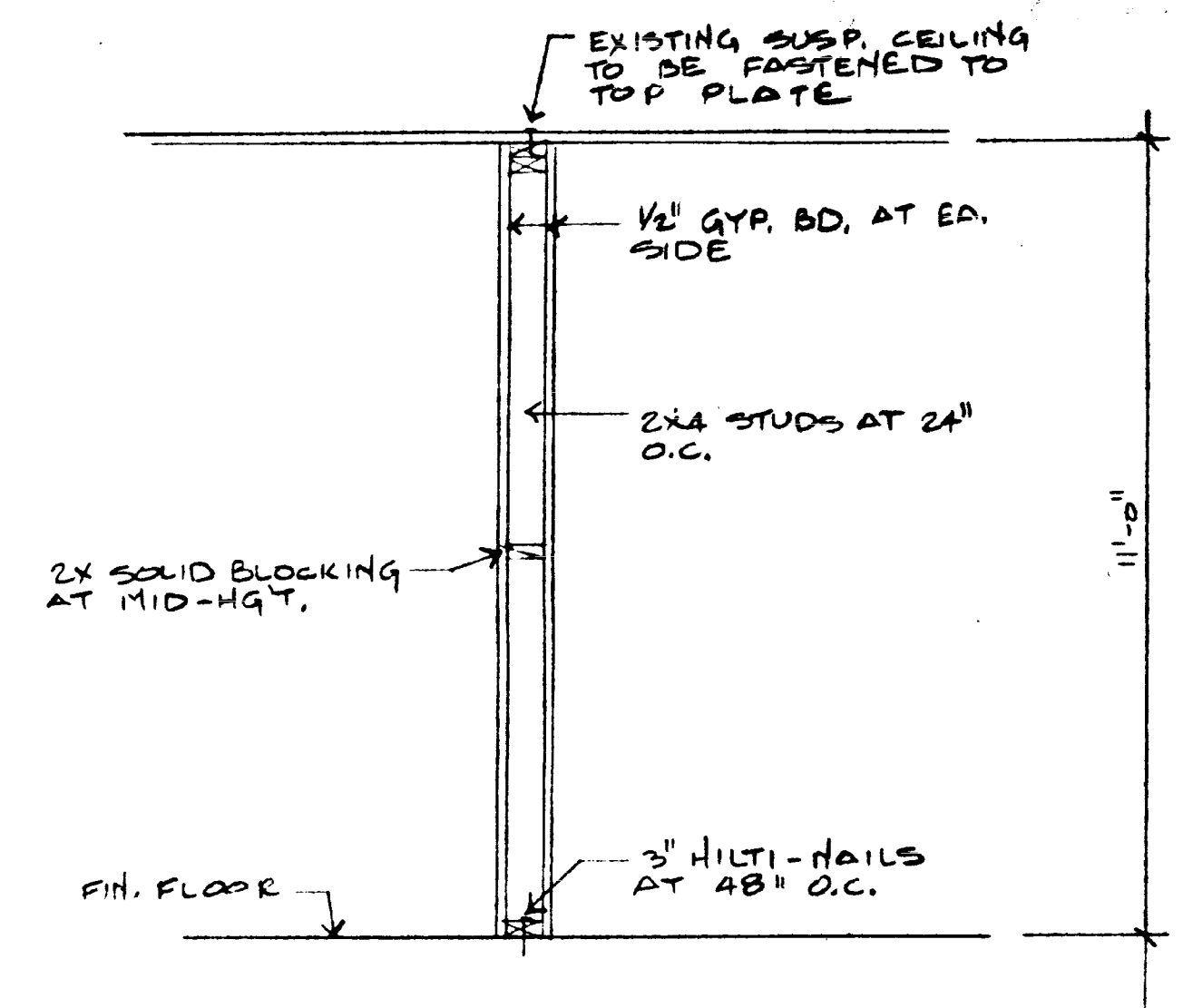
GENERAL DYNAMICS/ CONVAIR DIVISION  
IMPERIAL, CA

SCALE: \_\_\_\_\_ APPROVED BY: \_\_\_\_\_ DRAWN BY: \_\_\_\_\_  
DATE: \_\_\_\_\_ REVISED 4-14-85  
FLOOR PLAN

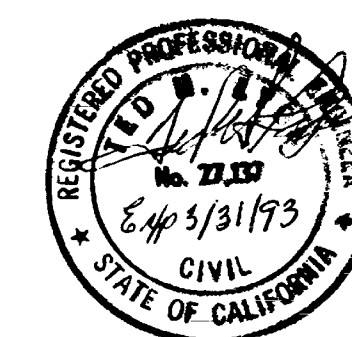
DRAWING NUMBER  
0 OF 18



ENLARGED FLOOR PLAN  
1/4" = 1'-0"



A TYP. WALL SECTION  
3/8" = 1'-0"



**DUGGINS CONSTRUCTION INC.**  
266 CEDAR ST., EL CENTRO, CA. 92243  
619-352-5600

GENERAL DYNAMICS / CONVAIR DIVISION IMPERIAL, CA.		APPROVED BY:	DRAWN BY: O.G.
SCALE: NOTED	DATE: 1-10-90	REVISOR:	REVISION:
ENLARGED FLOOR PLAN			DRAWING NUMBER 6 B OF 18

**BUILDING**  
COUNTY OF IMPERIAL

**INSPECTION**

**DIVISION**  
CALIFORNIA

**APPROVED**

ZONE..... PERMIT NO. *29789*

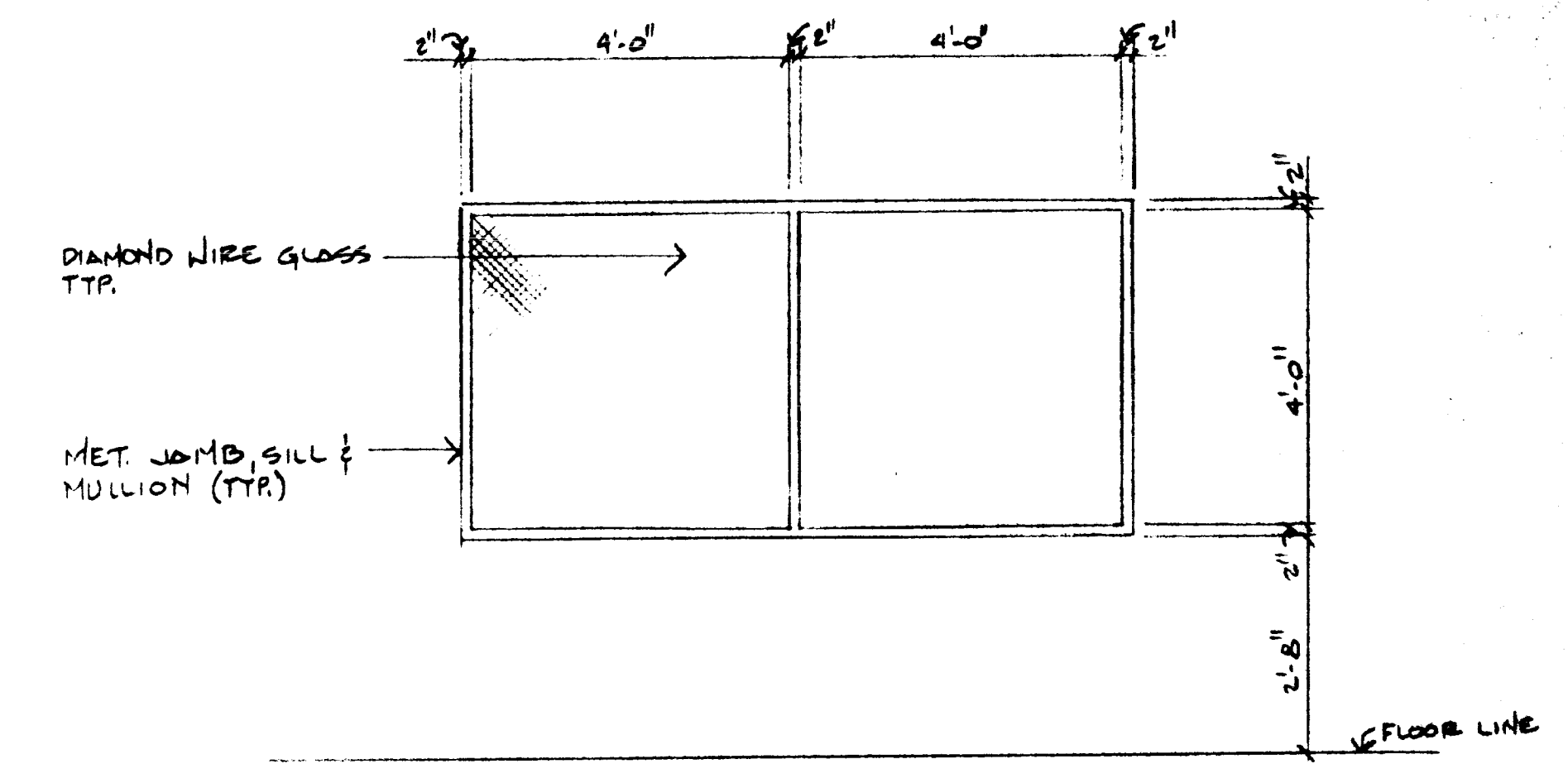
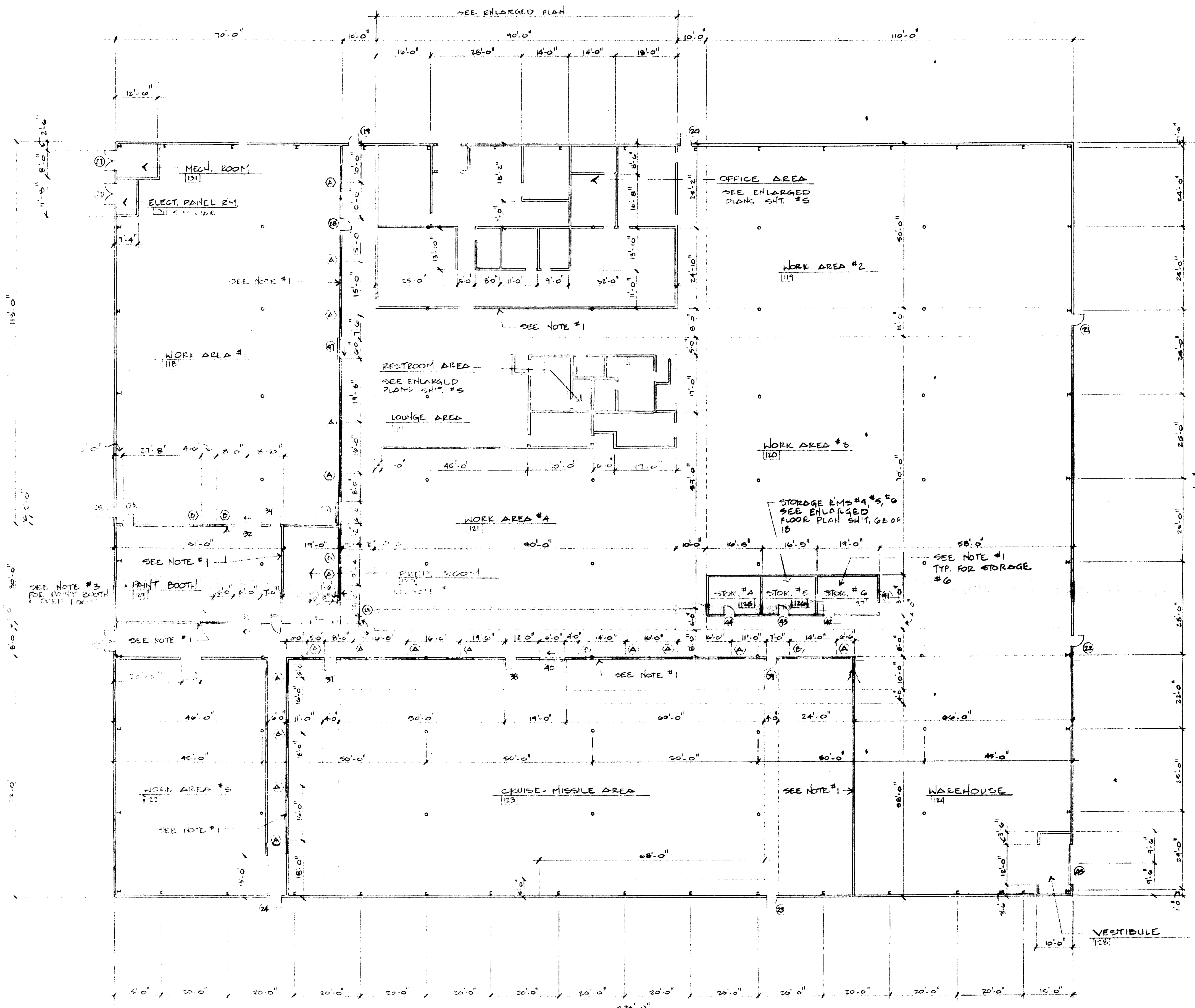
SETBACK: FRONT..... REAR..... SIDE.....

APPROVED BY: *John L. [Signature]* DATE *1-27-90*

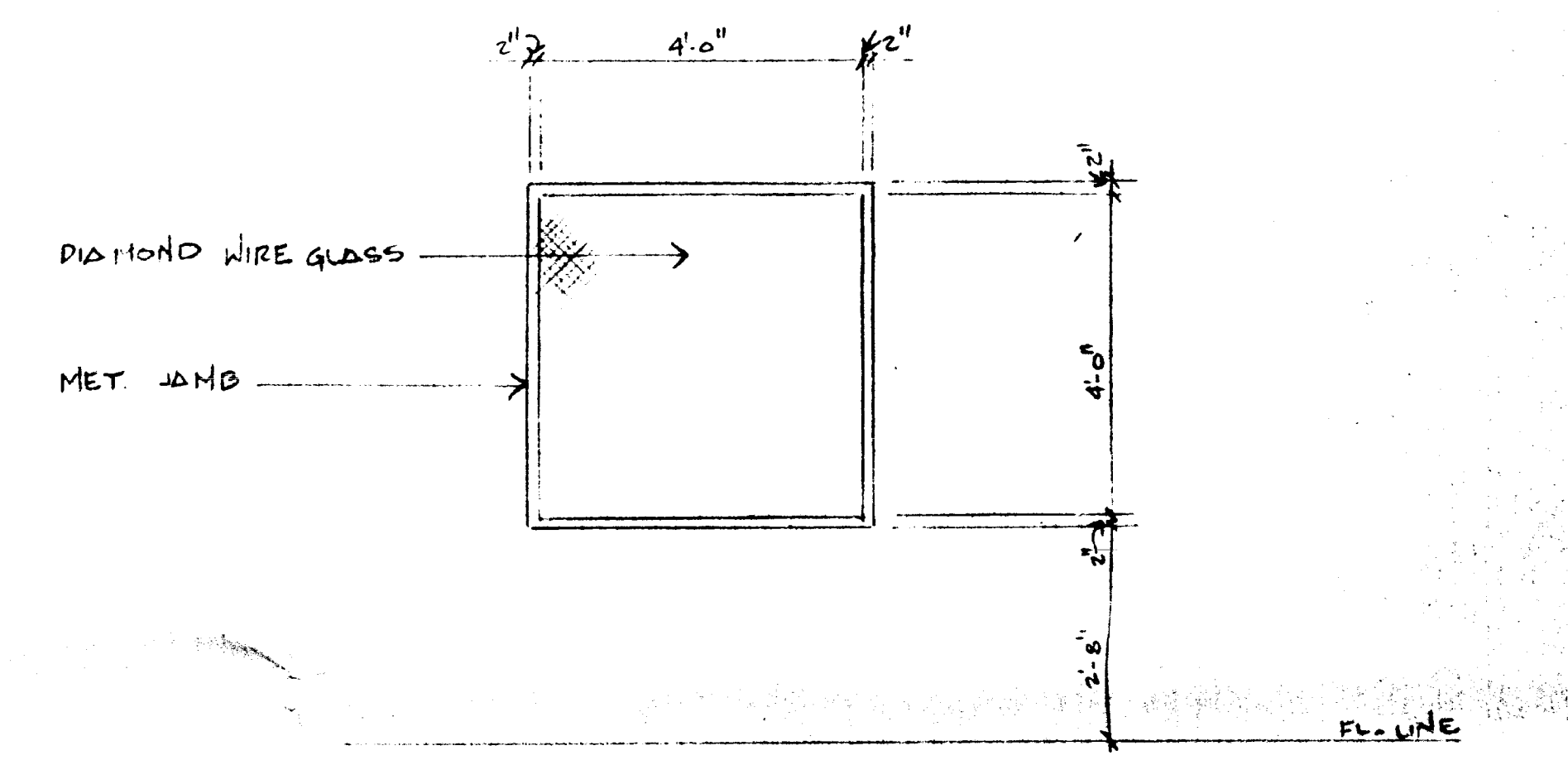
THIS IS NOT A BUILDING PERMIT

**OFFICE COPY**



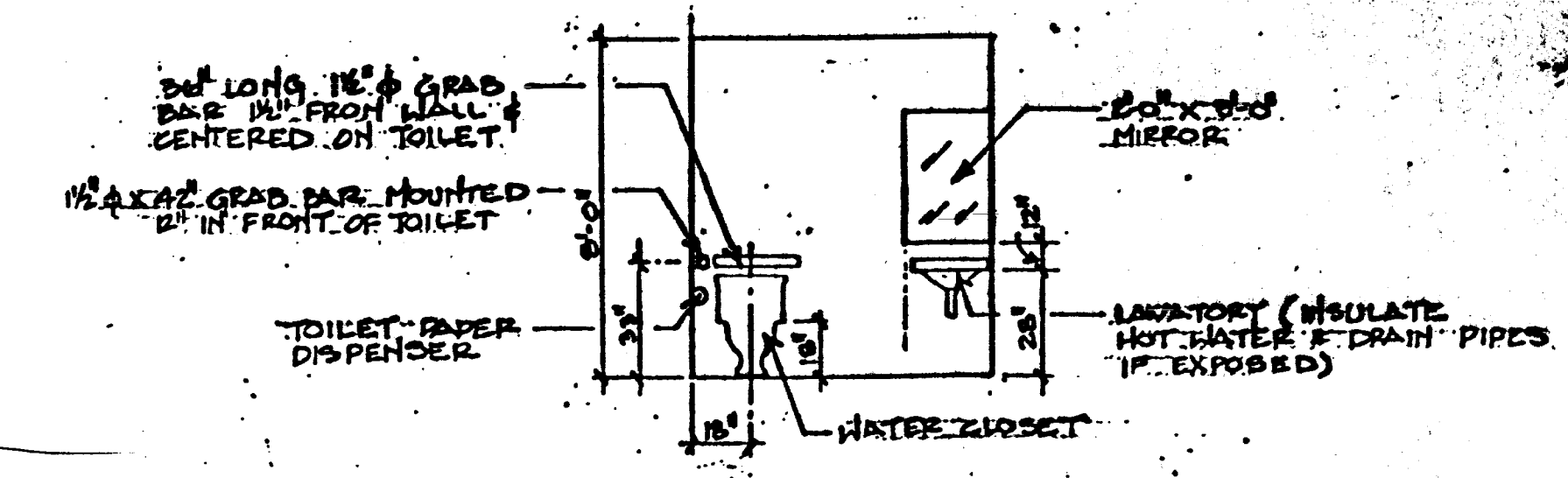


A DOUBLE WINDOW ELEVATION  
W = 11'-0"



B SINGLE WINDOW ELEVATION  
W = 11'-0"

NOTE: ALL WINDOWS AT FIRE RATED WALLS SHALL COMPLY WITH 1985 U.B.C. SECTION 4306 (1)



TYP TOILET ELEVATION  
SCALE: 1/4" = 1'-0"

FLOOR PLAN  
1/16" = 1'-0"

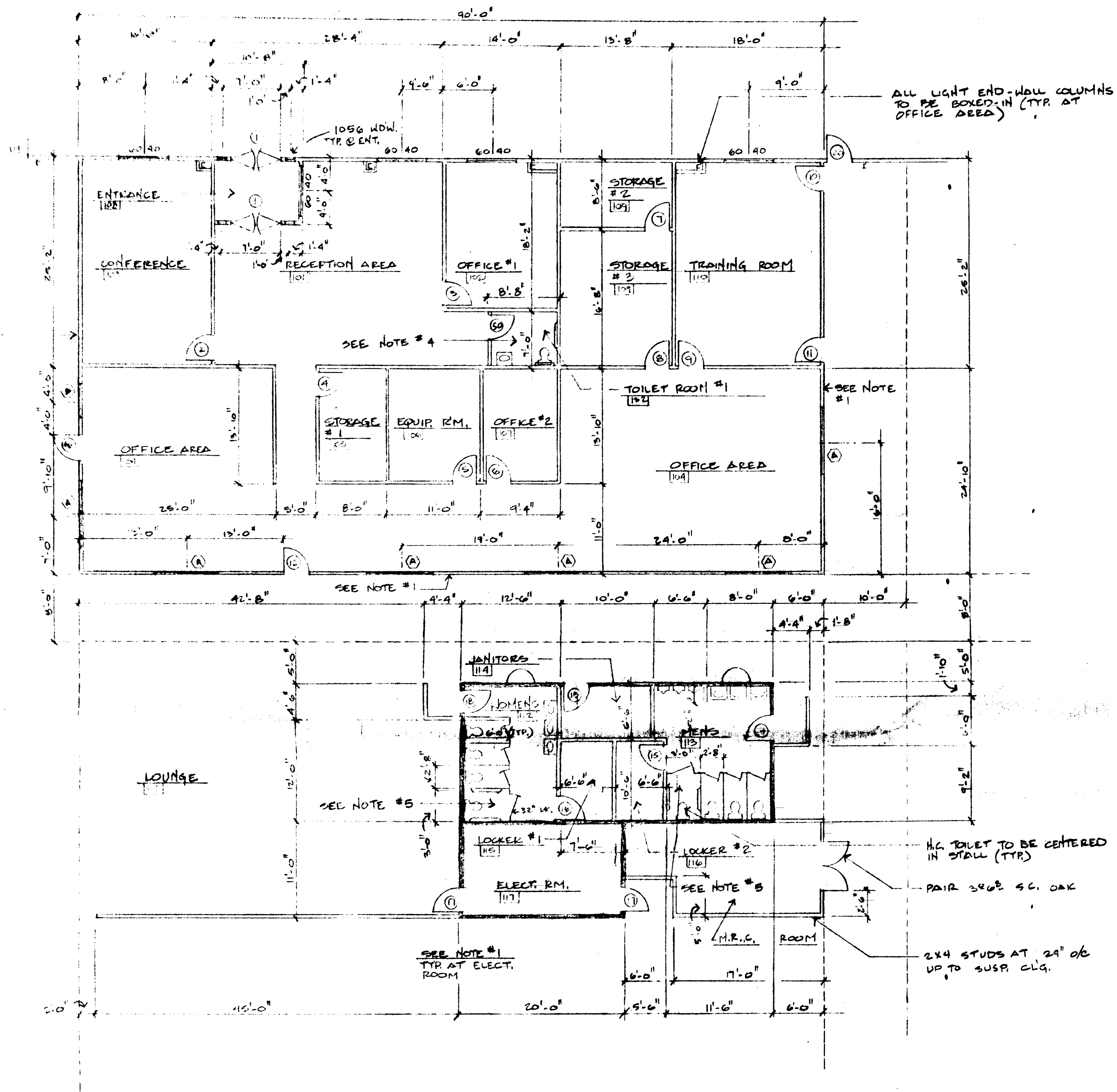
North

- NOTES:
- 2x6 FRAMED WALL @ 24" O.C. w/ 5/8" TYPE 'X' GYP. BO. AT ED. SIDES ARE REQUIRED FOR 1-HR. FIRE RATING
  - BUILDING TO BE FIRE SPRINKLED AS REQUIRED BY CODE
  - ROOMS FOR PAINT BOOTH TO BE PROVIDED PAINT BOOTH TO BE INSTALLED AT LATER DATE N.I.C.



**DUGGINS CONSTRUCTION INC.**  
266 CEDAR ST., EL CENTRO, CA 92243  
619-352-5600

GENERAL DYNAMICS / COMBAT DIVISION IMPERIAL, CA		DRAWN BY
SCALE:	APPROVED BY:	REVISION 4-14-85
DATE:		
FLOOR PLAN		DRAWING NUMBER
		6 OF 18



OFFICE & TOILET AREA - ENLARGED FLOOR PLAN  
SCALE: 1/8" = 1'-0"

- NOTES
- 2x6 FRAMED WALL @ 24" O.C. W/ 3/8" TYPE 'X' GYP. BD. AT EA. SIDE AS REQUIRED FOR 1-HR. FIRE RATING
  - ALL SHADDED WALLS TO BE 2x6 FRAMED WALLS AT 24" O.C. ALL OTHER WALLS TO BE 2x4 FRAMED WALLS UNLESS OTHERWISE SPECIFIED
  - TOILET ROOMS TO MEET REQUIREMENTS FOR HANDICAP ACCESSIBILITY
  - FOR TOILET FIXTURE LAYOUT & HANDICAP REQUIREMENTS SEE TYP. TOILET ELEVATION
  - FOR HANDICAP FIXTURE LAYOUT (ONLY) SEE TYP. TOILET ELEV.
  - FOR MISC. PLUMBING FIXTURE LOCATIONS SEE PLUMBING PLANS



ROOM FINISH SCHEDULE							
NO.	ROOM	FLOOR	BASE	WALLS	CEILING	CEILING HGT.	REMARKS
101	RECEPTION	CARPET	4" TOPSET	GYP. BD. T.T. & P.	2x4 SUSPENDED CEILING	10'-0"	
102	OFFICE #1						
103	CONFERENCE						
104	OFFICE AREA						
105	STORAGE #1						
106	EQUIP. ROOM						
107	OFFICE #2						
108	ENTRANCE	V.C. TILE					
109	STORAGE #2	CARPET					
110	TRAINING ROOM						
111	LOUNGE	V.C. TILE			2x4 SUSPENDED CEILING	12'-0"	
112	WOMEN'S				GYP. BD. T.T. & P.	10'-0"	
113	MEN'S						
114	JANITORS RM.						
115	LOCKER RM #1						
116	LOCKER RM #2						
117	ELECT. ROOM						
118	WORK AREA #1		NONE	W/BEAR PANEL	2x4 SUSPENDED CEILING	12'-0"	
119	WORK AREA #2						
120	WORK AREA #3						
121	WORK AREA #4						
122	WORK AREA #5						
123	ASSEMBLY AREA						
124	WAREHOUSE						
125	STORAGE #4		4" TOPSET	GYP. BD. T.T. & P.	GYP. BD. T.T. & P.		BY OTHERS
126	STORAGE #5						
127	STORAGE #6						
128	REST ROOM						
129	PAINT ROOM				2x4 SUSPENDED CEILING	12'-0"	
130	PREP. ROOM						
131	MECH. ROOM				GYP. BD. T.T. & P.	10'-0"	
132	TOILET RM #1					10'-0"	

DOOR SCHEDULE								
NO.	SIZE	THK.	MATERIAL	FINISH	MATERIAL	WIDTH	FINISH	REMARKS
1	PAIR 3'0" x 7'0"		GLASS	PRE-FIN.	STEEL		PAINT	23, 26, 9 TO HAVE VU-LITE
2-7	3'0" x 6'0"		S.C. BIRCH	STAIN & LAQUER		4 1/2"		10, 11, 12 TO HAVE VU-LITE 1-HR. RATED DOORS
10-12	3'0" x 6'0"		S.C. BIRCH			6 3/4"		
13-14	3'0" x 6'0"		S.C. BIRCH			6 1/2"		
15-16	2'0" x 6'0"		S.C. BIRCH			4 1/2"		
17-18	3'0" x 6'0"		S.C. BIRCH			*17 - 6 3/4" *18 - 6 1/2"		(*17) 1-HR. RATED DOOR BY METAL BLDG. MANUFACTURER
19-25	3'0" x 7'0"		STEEL	PRE-FIN.				
26-27	PAIR 3'0" x 7'0"		STEEL					
28-30	3'0" x 6'0"		S.C. BIRCH	STAIN & LAQUER		6 3/4"		1-HR. RATED DOOR
31-32	6'0" x 7'0"		SLIDER					
33-34	3'0" x 6'0"		S.C. BIRCH	STAIN & LAQUER				
35-36	6'0" x 7'0"		SLIDER					
37-39	3'0" x 6'0"		S.C. BIRCH					
40	6'0" x 7'0"		SLIDER					
41-44	3'0" x 6'0"		S.C. BIRCH	STAIN & LAQUER		6 3/4"		
45	12'0" x 16'0"		STEEL					DR. #40 TO HAVE VU-LITE 1-HR. RATED DOOR
46	3'0" x 6'0"		S.C. BIRCH	STAIN & LAQUER		6 3/4"		
47-48	6'0" x 7'0"		SLIDER					
49	3'0" x 6'0"		S.C. BIRCH	STAIN & LAQUER				
50	3'0" x 6'0"		S.C. BIRCH			4 1/2"		

**DUGGINS CONSTRUCTION INC.**  
266 CEDAR ST., EL CENTRO, CA 92523  
619-352-5600

GENERAL DYNAMICS/ CONVAIR DIVISION  
IMPERIAL, CA.

SCALE: NOTED  
DATE: \_\_\_\_\_  
ENLARGED OFFICE & TOILET AREAS

APPROVED BY: \_\_\_\_\_  
DRAWN BY: D.S.  
REVISOR: A.S.  
DRAWING NUMBER: 7 of 18

**BUILDING INSPECTION DIVISION**  
COUNTY OF IMPERIAL CALIFORNIA

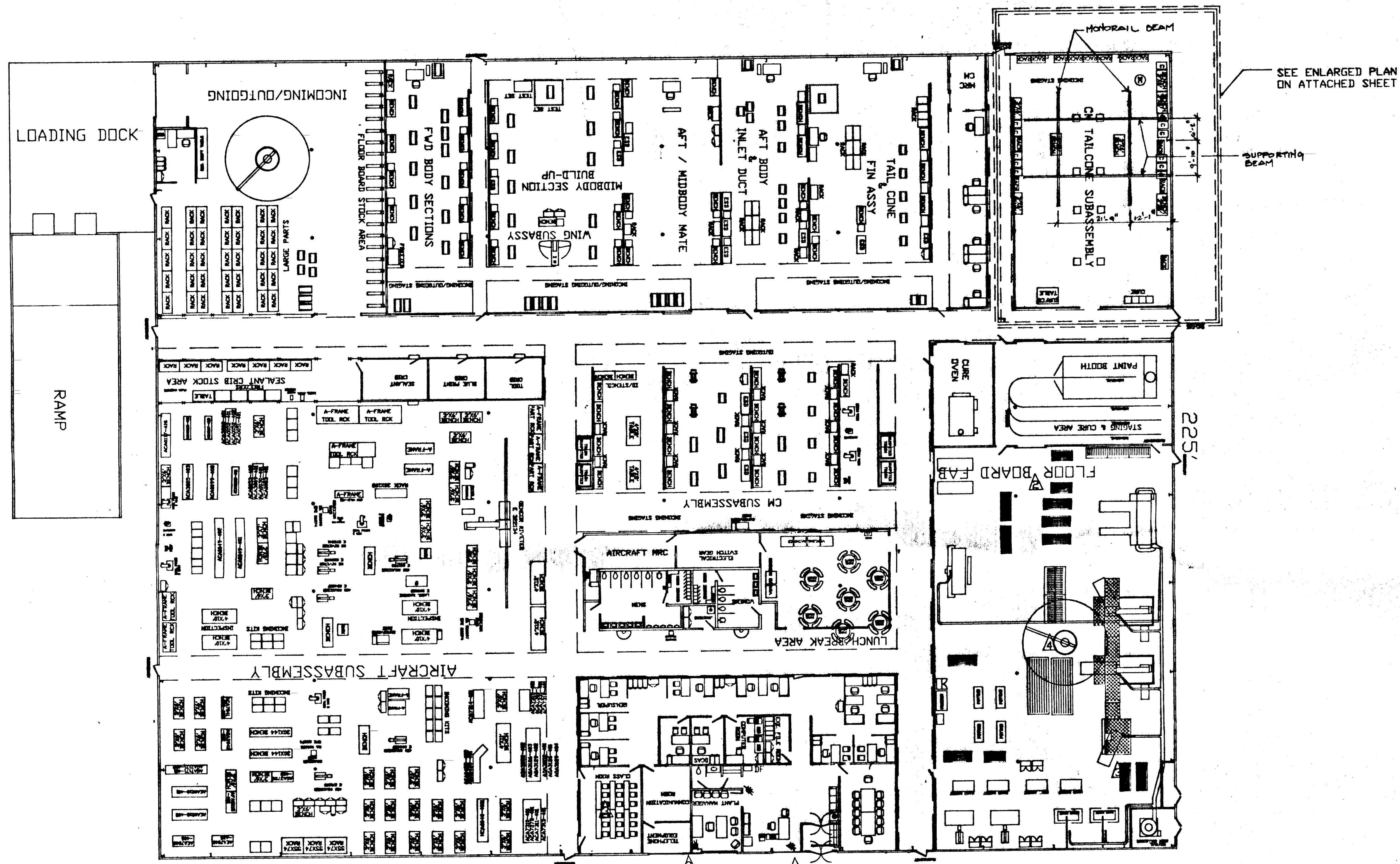
**APPROVED**

ZONE \_\_\_\_\_ PERMIT NO. *2A719*  
SETBACK: FRONT \_\_\_\_\_ BEAR \_\_\_\_\_ SIDE \_\_\_\_\_  
APPROVED BY: *[Signature]* DATE *1-27-90*

THIS IS NOT A BUILDING PERMIT

**OFFICE COPY**





SEE ENLARGED PLAN ON ATTACHED SHEET

225'

FLOOR PLAN  
1/8" = 1'-0"

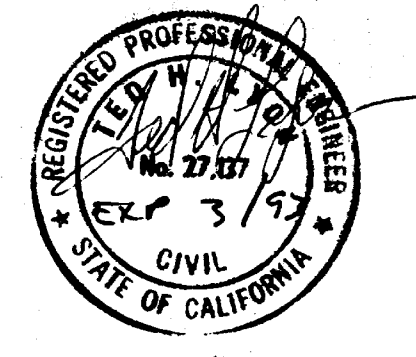


EXHIBIT 'B'

IMPERIAL DYNAMICS  
Convair Division

PLANT SERVICES

TITLE: IMPERIAL VALLEY FACILITY OCT 3, 1989

SUB-TITLE: FLOOR PLAN F-1

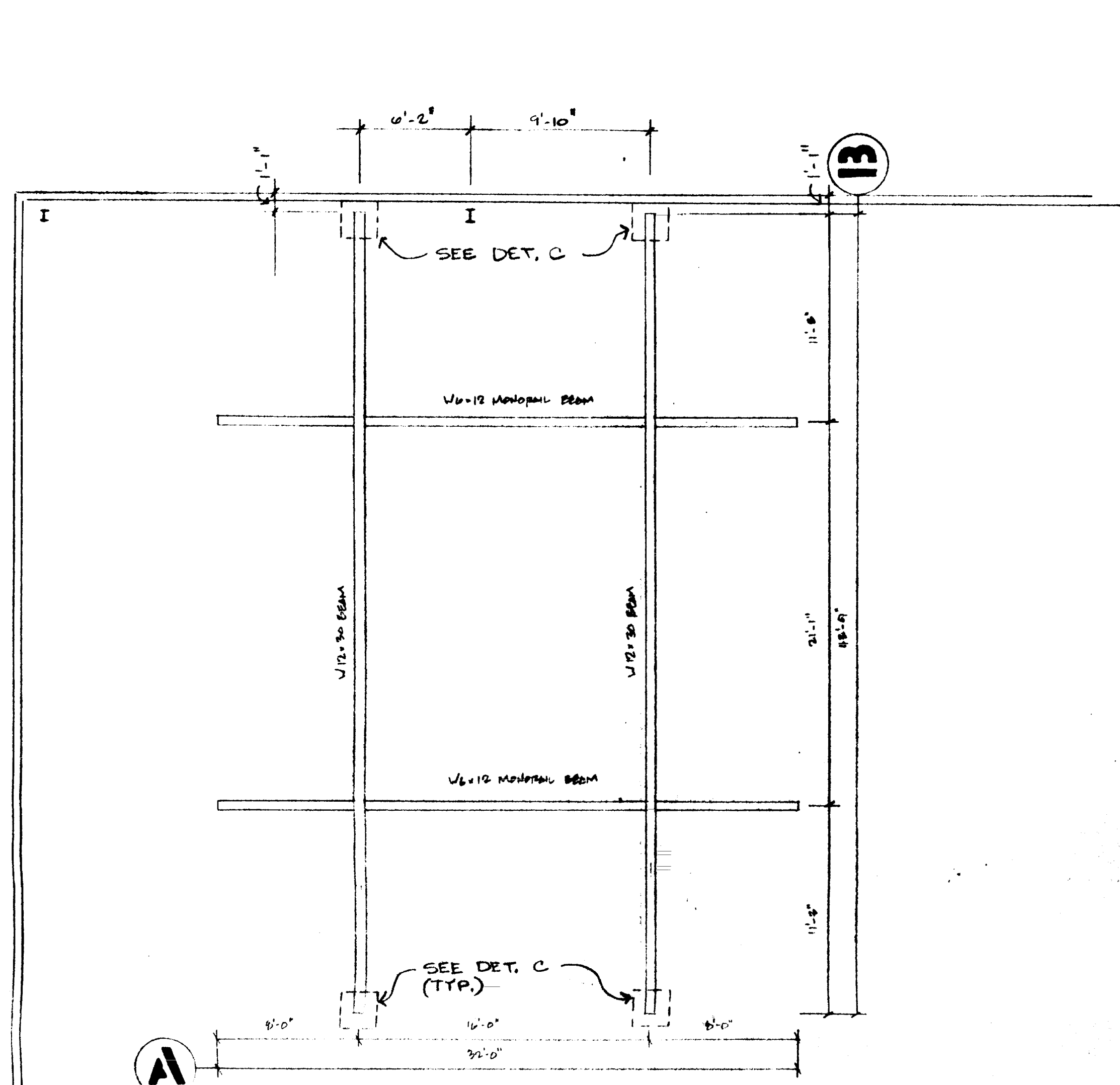
SCALE: 1/8" = 1'-0" NUMBER: 4-8263 SHEET: REV: N

SYL	REVISIONS	DATE	APPROVALS			PREPARED BY CARL BROWN	DATE
			SIGNATURE	DEPT.	DATE		
1						CHECKED BY DAVE SNYDER	
2	ADD WINDOW FRONT VEST, 10 HR DOORS	11/28/89				ENGINEER CARL BROWN	
3	ADD SOUND INSULATION TO WALLS	11/28/89					
4	RELOCATE CURE OVEN ENCLOSURE	11/28/89					
						SUPERVISOR DAVE SNYDER	
						AUTHORIZATION	QTY. DATE

LEGEND

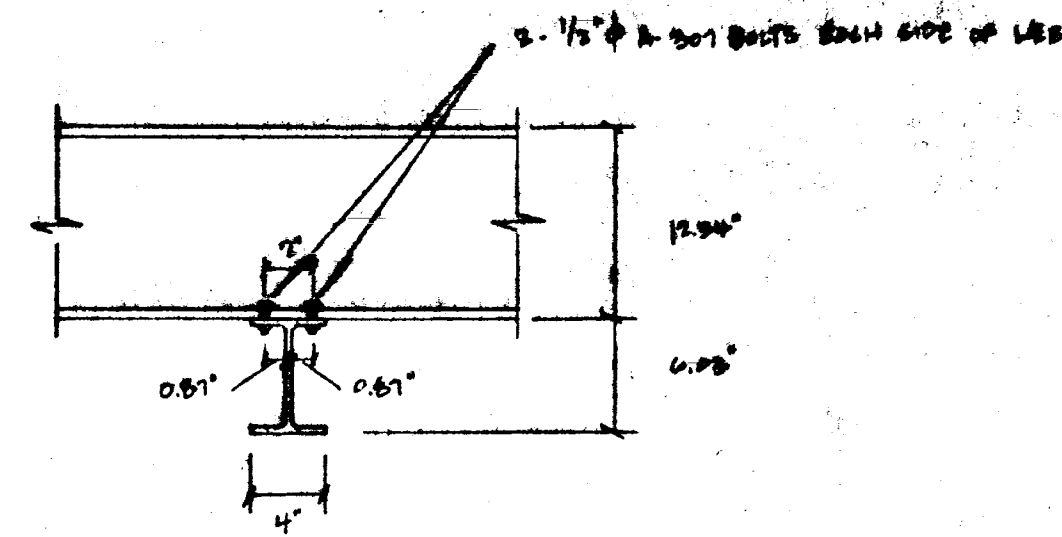
RECEIVED  
JAN 17 1990  
IMPERIAL COUNTY  
BUILDING DIVISION



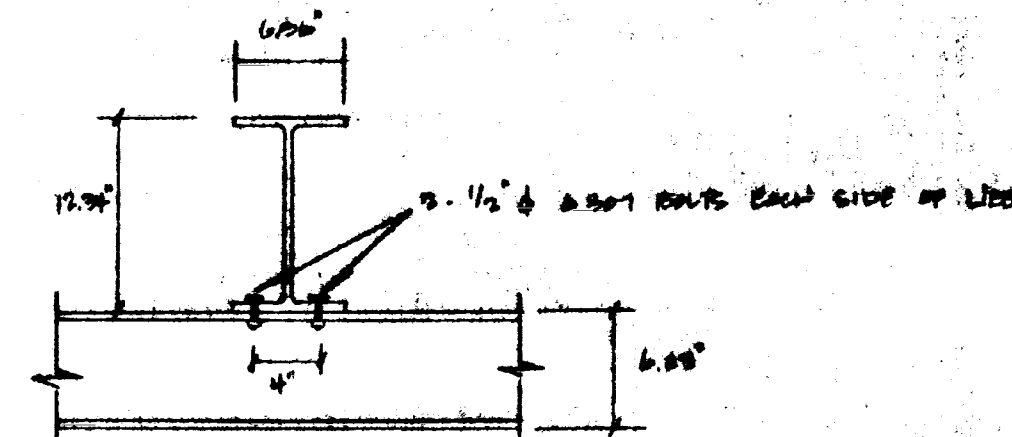


FLOOR PLAN (ENLARGED)

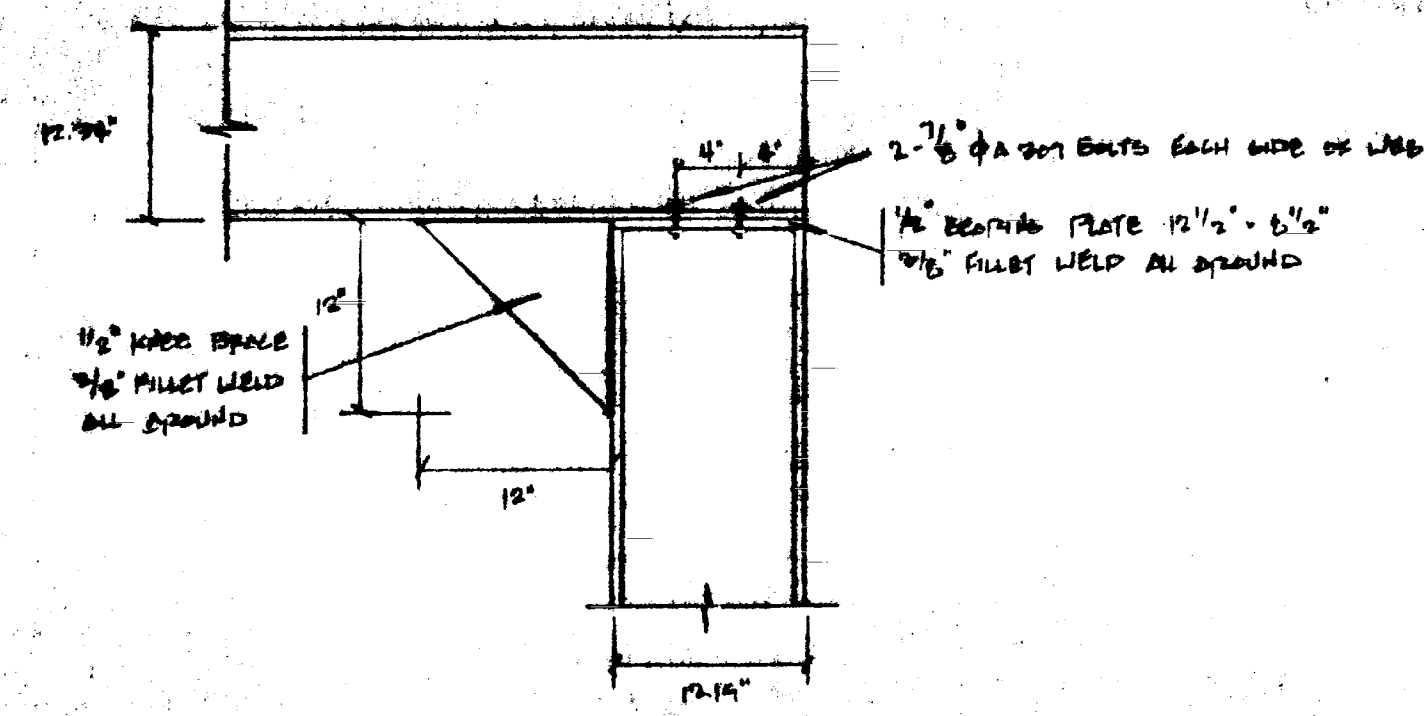
3/16" = 1'-0"



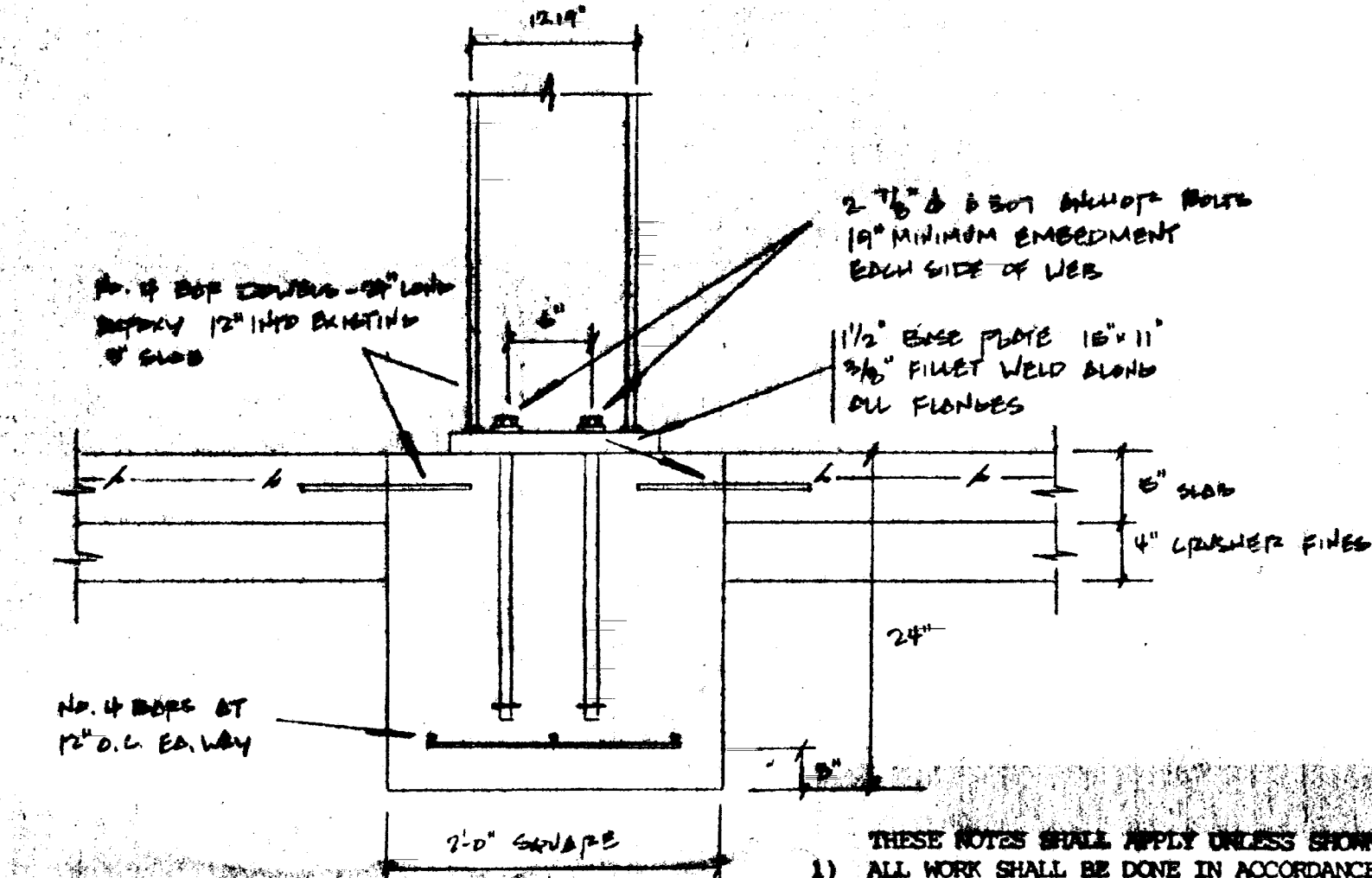
DETAIL D



DETAIL A



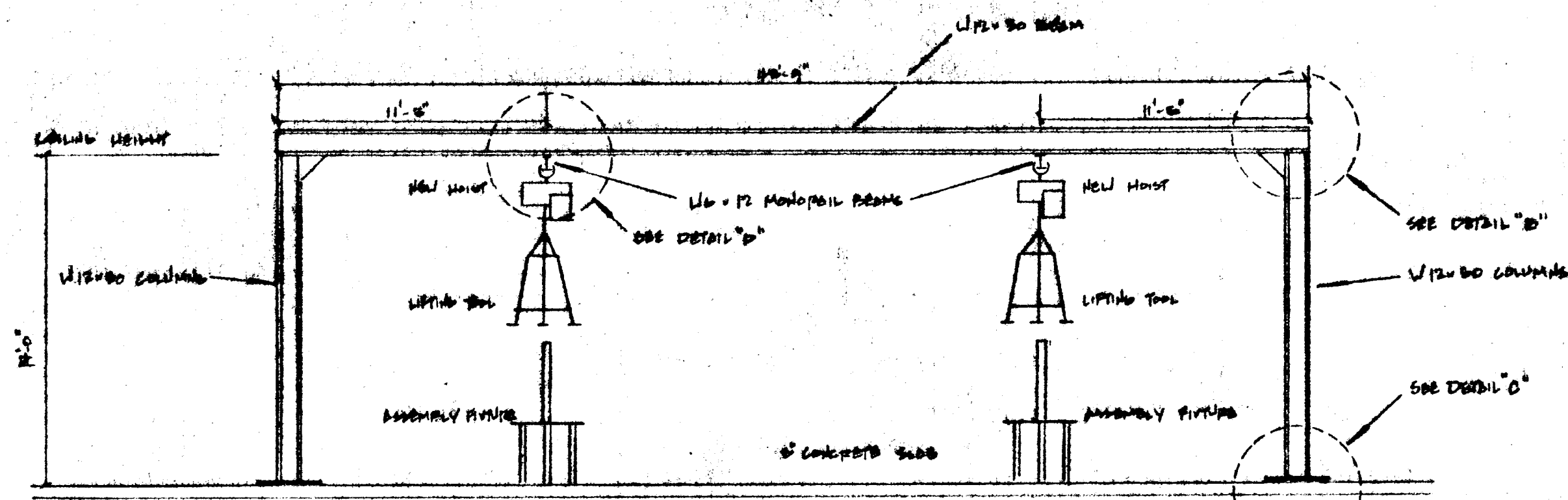
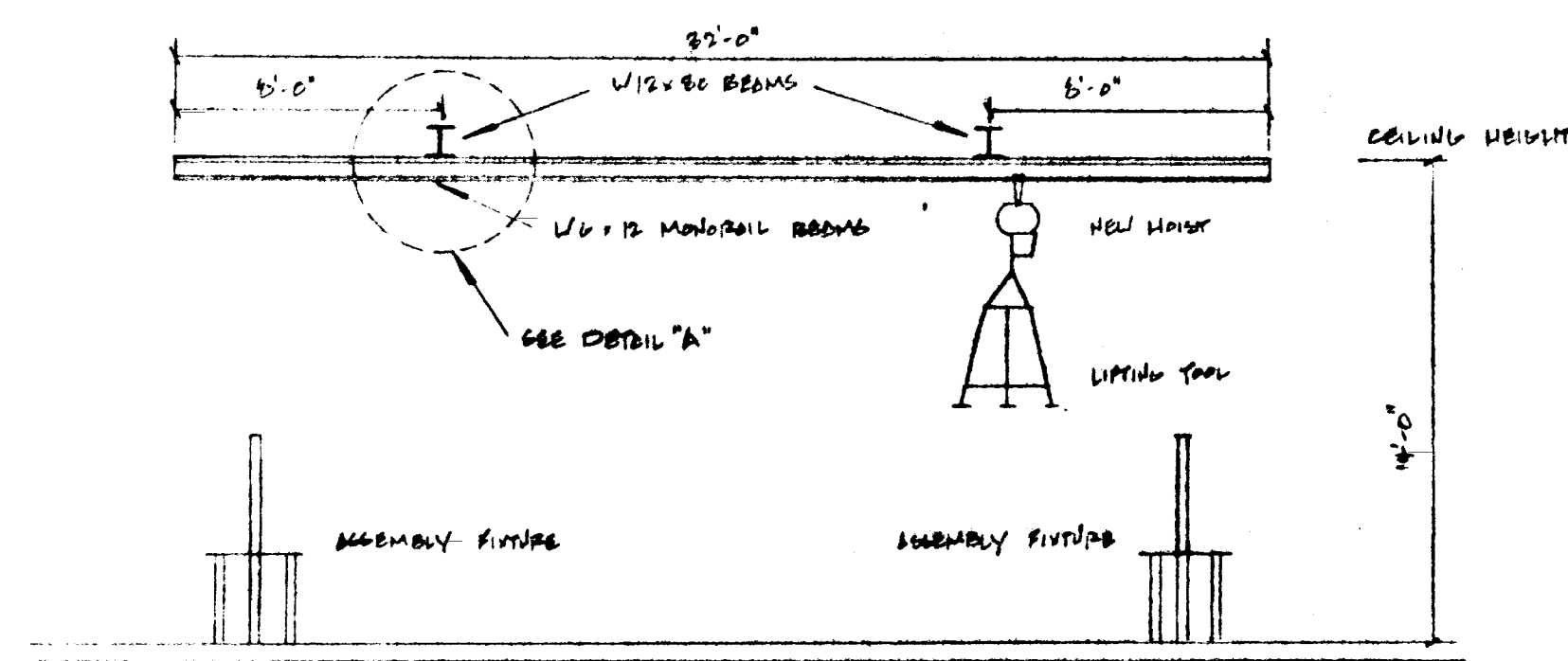
DETAIL B



DETAIL C

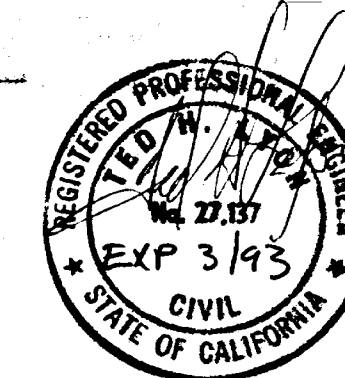
GENERAL NOTES

- 1) THESE NOTES SHALL APPLY UNLESS SHOWN OTHERWISE ON PLANS.
- 2) ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE LATEST EDITION OF THE UNIFORM BUILDING, PLUMBING, MECHANICAL AND FIRE CODES AND/OR LOCAL CODES AND ORDINANCES.
- 3) THE ENGINEER SHALL NOT BE RESPONSIBLE FOR CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES, PROCEDURES OR FOR SAFETY PRECAUTIONS AND PROGRAMS OR FOR THE CONTRACTOR'S FAILURE TO PERFORM THE WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS.
- 4) THE ENGINEER SHALL NOT BE RESPONSIBLE FOR THE ACTS OR OMISSIONS OF THE CONTRACTOR OR ANY SUBCONTRACTOR OR ANY OF THEIR AGENTS OR EMPLOYEES OR ANY OTHER PERSON PERFORMING ANY OF THE WORK.
- 5) IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO ADEQUATELY SUPPORT ALL WALLS, TRUSSES, ETC. DURING CONSTRUCTION. THE CONTRACTOR SHALL DESIGN, CONSTRUCT AND MAINTAIN ALL SAFETY DEVICES, INCLUDING SHORING AND BRACING AND SHALL BE SOLELY RESPONSIBLE FOR CONFORMING TO ALL LOCAL, STATE AND FEDERAL SAFETY AND HEALTH STANDARDS, LAWS AND REGULATIONS.
- 6) THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS, ELEVATIONS AND SITE CONDITIONS BEFORE STARTING WORK, AND SHALL NOTIFY THE ENGINEER IMMEDIATELY OF ANY DISCREPANCIES.
- 7) NOTES AND DETAILS ON THE DRAWINGS SHALL TAKE PRECEDENCE OVER THESE GENERAL NOTES IN CASE OF CONFLICT.
- 8) WHERE NO CONSTRUCTION DETAILS ARE SHOWN OR NOTED FOR ANY PART OF THE WORK, SUCH DETAILS SHALL BE THE SAME AS FOR SIMILAR WORK SHOWN ON THE DRAWINGS.
- 9) SHOULD DISCREPANCIES OCCUR ON THESE PLANS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY.
- 10) THESE DRAWINGS ARE NOT TO BE SCALED FOR DIMENSIONAL INFORMATION. ACTUAL DIMENSION NUMBERS SHOWN ON THE PLANS ARE TO BE USED. IF DIMENSIONAL CONFLICTS EXIST, NOTIFY THE ENGINEER IMMEDIATELY. ANY CONTRACTOR SCALING DRAWINGS FOR ANY PURPOSE DOES SO AT HIS OWN RISK AND IS RESPONSIBLE FOR ANY ERRORS AND MISTAKES WHICH MAY RESULT.
- 11) NO TOPOGRAPHIC OR GRADING INFORMATION HAS BEEN PROVIDED FOR THIS SITE. THEREFORE NO FINISHED FLOOR ELEVATION HAS BEEN SHOWN ON THE PLAN. FINISHED FLOOR ELEVATION SHALL BE ESTABLISHED BY A PROPERLY PREPARED GRADING PLAN OR BY LOCAL CODES.
- 12) ALL STRUCTURAL STEEL SHALL BE A-36 STEEL WITH  $F_y = 36,000$  PSI.



REINFORCED CONCRETE

- 1) WELDED WIRE FABRIC SHALL CONFORM TO A.S.T.M. SPEC. A-185 AND BE PLANT SHEETS.
- 2) ANCHOR BOLTS SHALL BE GRADE "A" CONFORMING TO A.S.T.M. SPEC. A-307.
- 3) CONCRETE MIXES SHALL CONFORM TO THE UNIFORM BUILDING CODE AND ATTAIN A MINIMUM COMPRESSIVE STRENGTH OF 2000 P.S.I. AT 28 DAYS CURE AND CONTAIN 5 BAGS OF CEMENT PER CUBIC YARD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE IF ANY SPECIAL ADMIXTURES ARE REQUIRED FOR THIS JOB FOR ITEMS SUCH AS: IMPROVE DURABILITY, WATER REDUCTION, RETARDING SETTING TIME, ACCELERATING SETTING TIME, TEMPERATURE CONTROL, ELIMINATE ADVERSE EFFECTS OF REACTIVE AGGREGATES, IMPROVE WORKABILITY, PROVIDE WATER TIGHTNESS OR PROVIDE EXPANSION PRIOR TO HARDENING.
- 4) ALL REINFORCING STEEL SHALL BE ADEQUATELY SUPPORTED BY METAL CHAIRS, SPACERS, ETC. BEFORE CONCRETE PLACING IS BEGUN.
- 5) ALL REINFORCING STEEL SHALL BE DEFORMED BARS OF GRADE 40 STEEL (OR BETTER) CONFORMING TO A.S.T.M. SPEC. A-615 (F<sub>y</sub> = 40 KSI).
- 6) LAP SPLICE CONTINUOUS REINFORCING STEEL 40 BAR DIAMETERS IN CONCRETE, OR 24 INCHES, WHICHEVER IS GREATER. STAGGER ALL SPLICES.
- 7) JOINTS: THE LOCATION OF ALL CONSTRUCTION, CONTROL AND WEAKENED PLANE JOINTS NOT SPECIFICALLY INDICATED ON THE DRAWINGS SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE PLACING OF THE CONCRETE.
- 8) MINIMUM PROTECTIVE COVER FOR REINFORCING STEEL:  
ON EARTH SIDE WHEN PLACED AGAINST EARTH.....3"  
ON EARTH SIDE WHEN FORMED.....2"  
..... CENTER LINE OF SLAB  
SLAB ON GRADE STEEL.....2"
- 9) FOOTINGS SHALL REST ON FIRM UNDISTURBED NATURAL SOIL OR PROPERLY COMPACTED FILL.
- 10) LYON ENGINEERS, INC. HAS NOT BEEN PROVIDED WITH SPECIAL DESIGN LOAD INFORMATION FOR THE CONCRETE SLAB. IT SHALL BE DURING CONSTRUCTION, INC. RESPONSIBILITY TO VERIFY WITH THE OWNER THAT THE CONCRETE SLAB SHOWN IS ADEQUATE FOR THE INTENDED USE.
- 11) SEE METAL BUILDING MANUFACTURING DRAWINGS FOR ANCHOR BOLT SIZE AND LOCATION.
- 12) IF THE SOIL AT THE SITE IS EXPANSIVE, THE FOUNDATION AREA SHALL BE PROTECTED TO A MOISTURE CONTENT AND A LENGTH OF TIME BEFORE THE POUR AS NECESSARY FOR THE SPECIFIC SOIL CONDITIONS.



<b>Lyon Engineers, Inc.</b>		122 STATE STREET EL CENTRO, CALIFORNIA 92243 818-353-8110	
CIVIL ENGINEERING		General Dynamics	
Imperial County, California		DATE: 1/16/93	SHEET: 1 OF 1
CLIENT: Submarine Control, Inc.		JOB: 3-2-1	

**BUILDING**  
COUNTY OF IMPERIAL

**INSPECTION**

**DIVISION**  
CALIFORNIA

**APPROVED**

ZONE..... PERMIT NO. *29789*

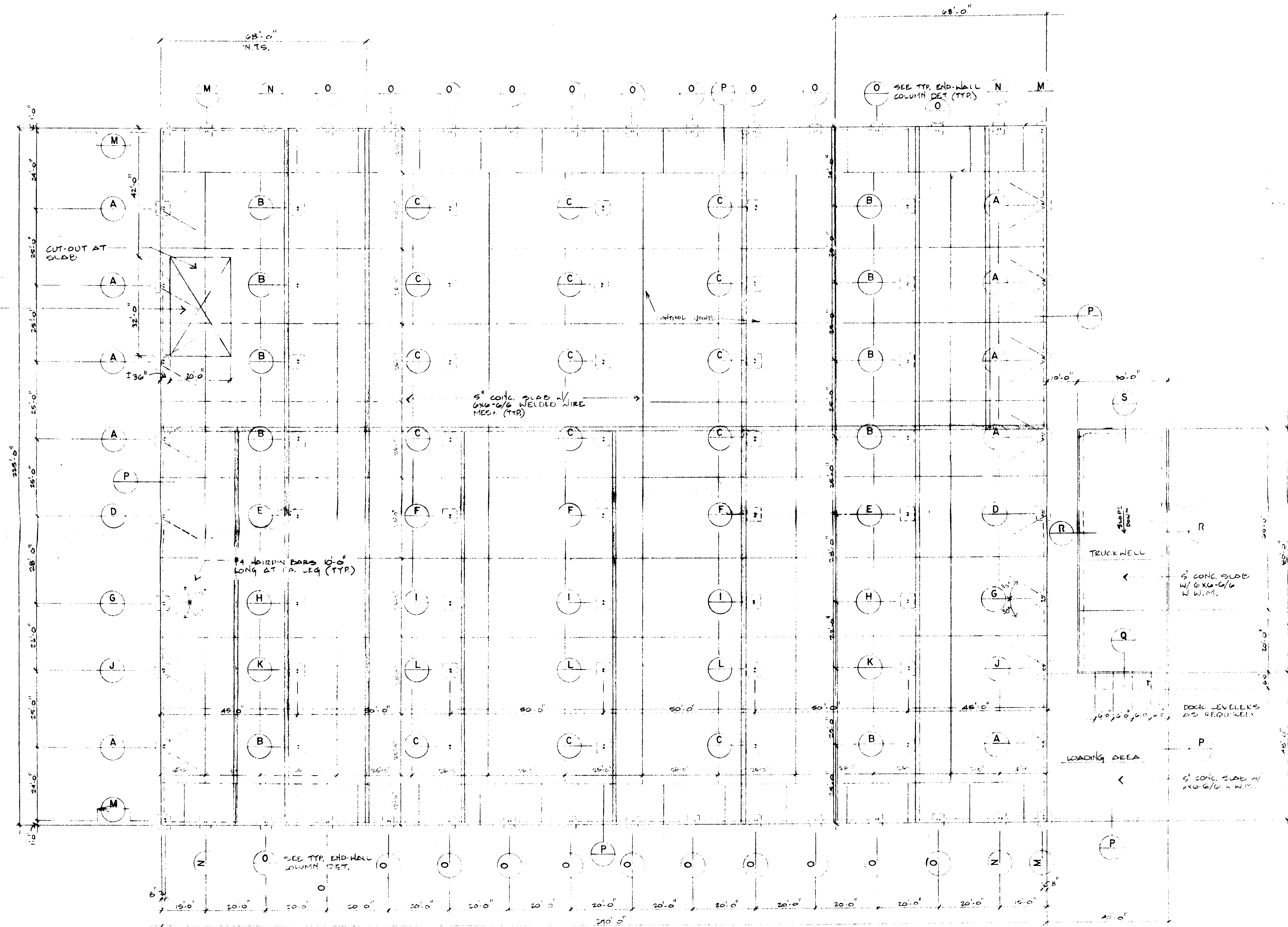
SETBACK: FRONT..... REAR..... SIDE.....

APPROVED BY: *John L. [Signature]* DATE *1-26-90*

THIS IS NOT A BUILDING PERMIT.

**OFFICE COPY**

SEE ENLARGED  
PLAN FOR THIS  
AREA



NOTE:  
 1. ALL DIMENSIONS ARE TO FACE UNLESS NOTED OTHERWISE.  
 2. ALL WALLS AND SLABS TO BE CONCRETE UNLESS NOTED OTHERWISE.  
 3. ALL REINFORCING TO BE #4 UNLESS NOTED OTHERWISE.

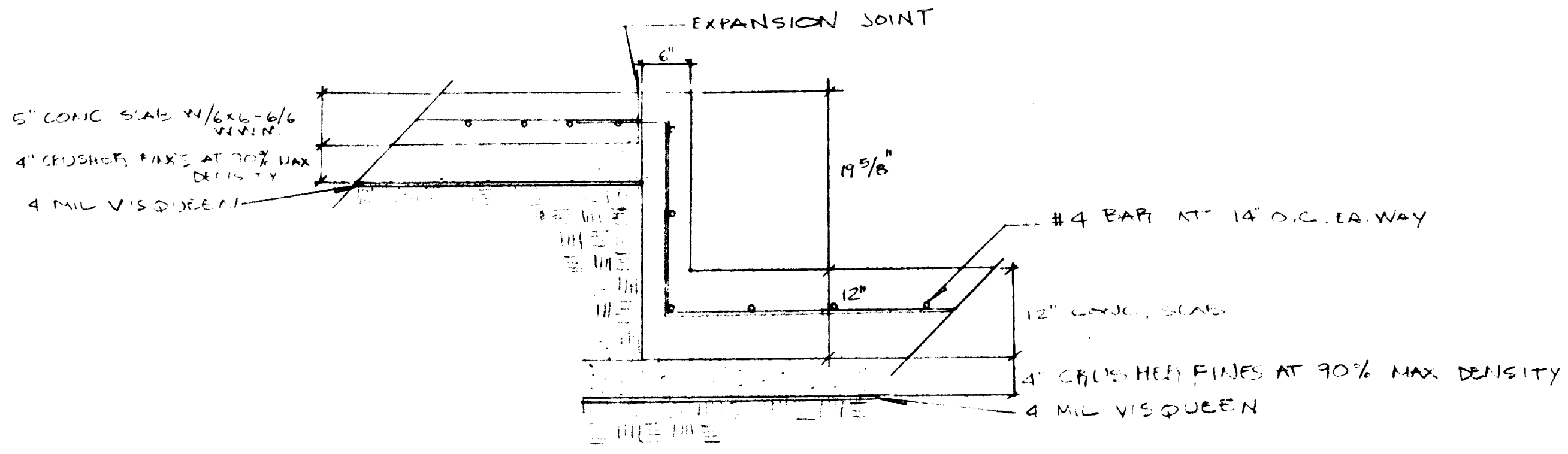
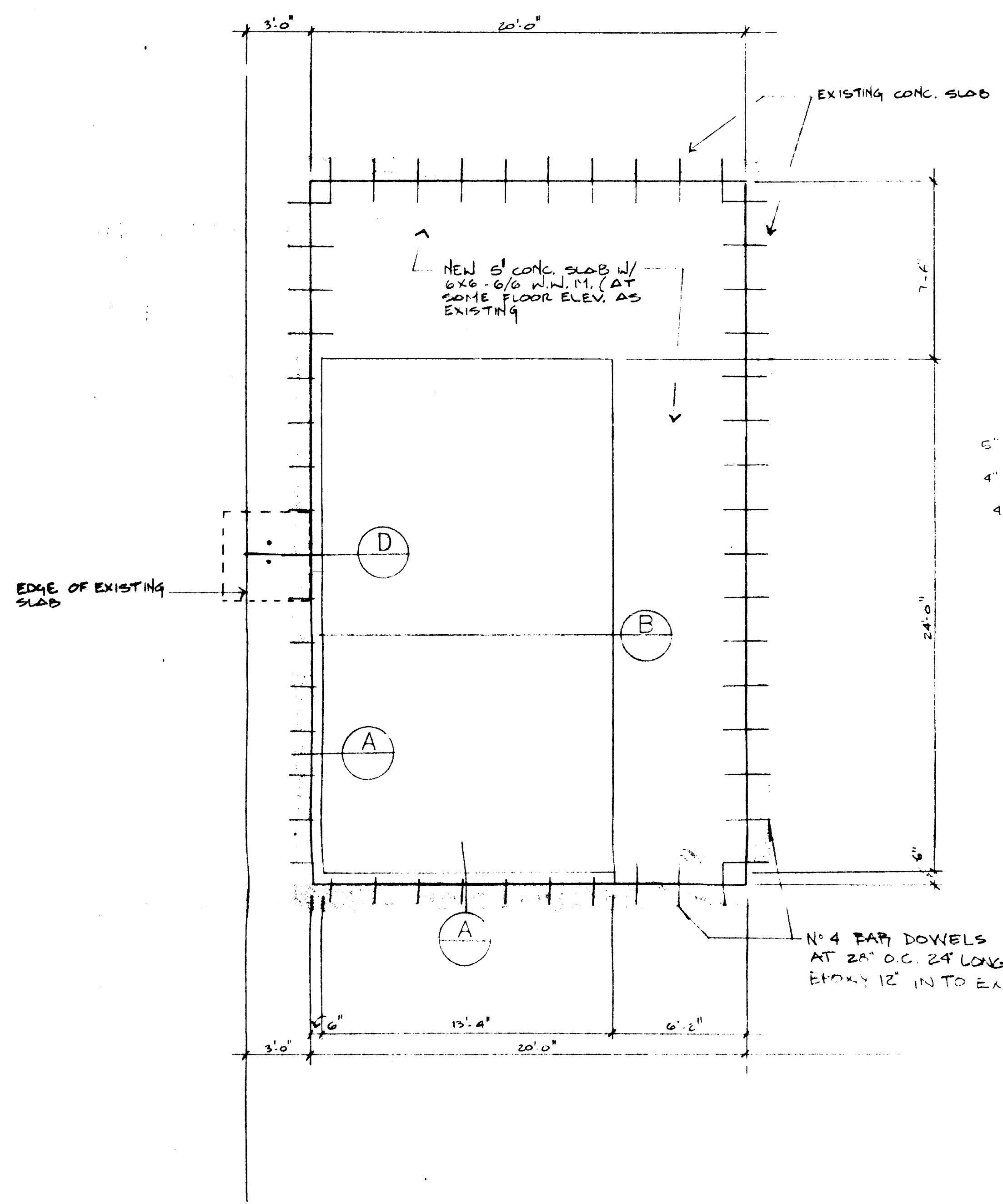
FOUNDATION PLAN  
 SCALE: 1/8" = 1'-0"



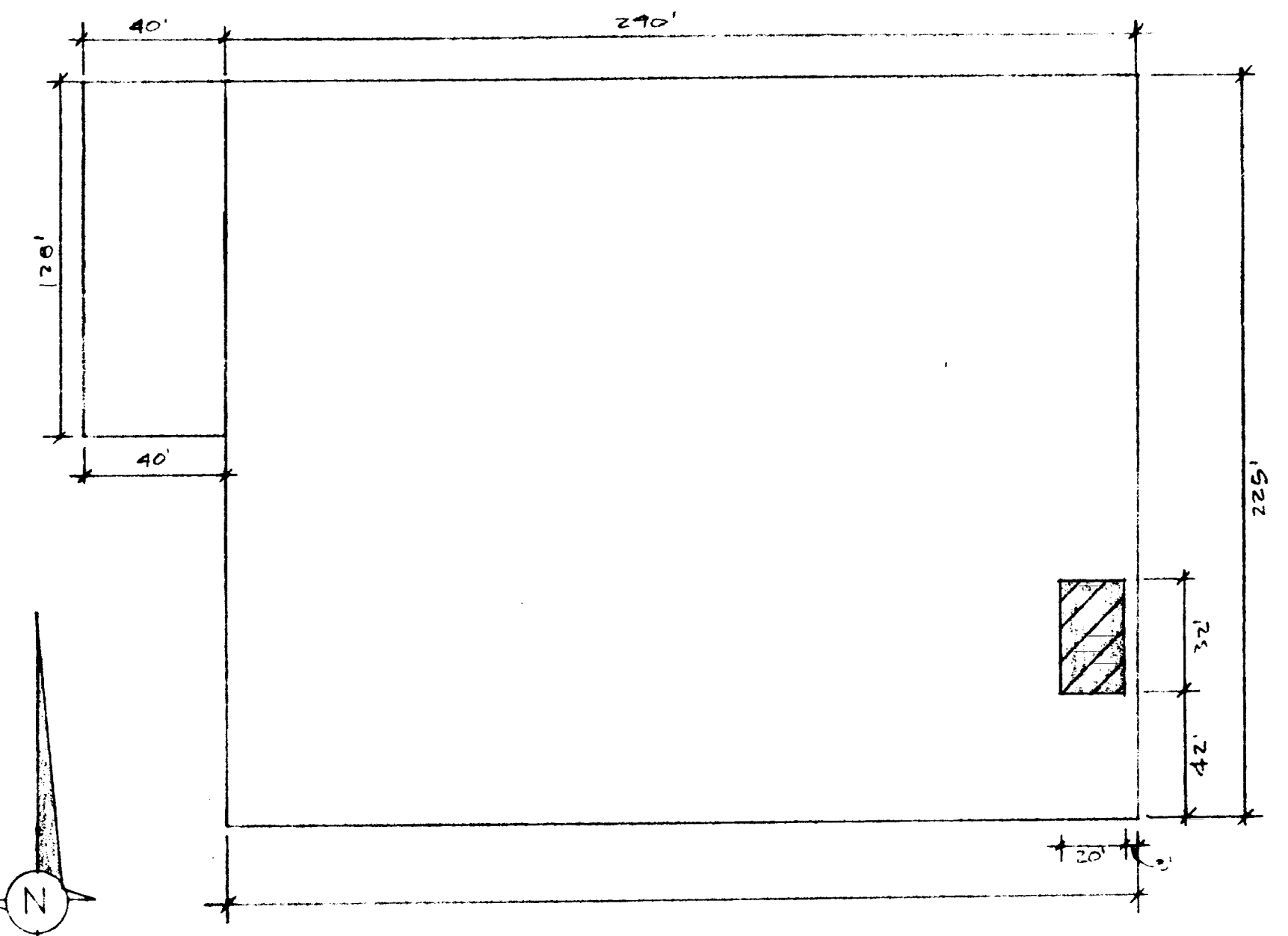
**DUGGINS CONSTRUCTION INC.**  
 240 CEDAR ST. EL CENTRO, CA 92543  
 619-352-5600

GENERAL DYNAMICS / CONVAIR DIVISION INPERVAL, CA.		
SCALE NOTED	APPROVED BY	DRAWN BY DS
		REVISION 3/73
FOUNDATION PLAN		DRAWING NUMBER
		4 of 13

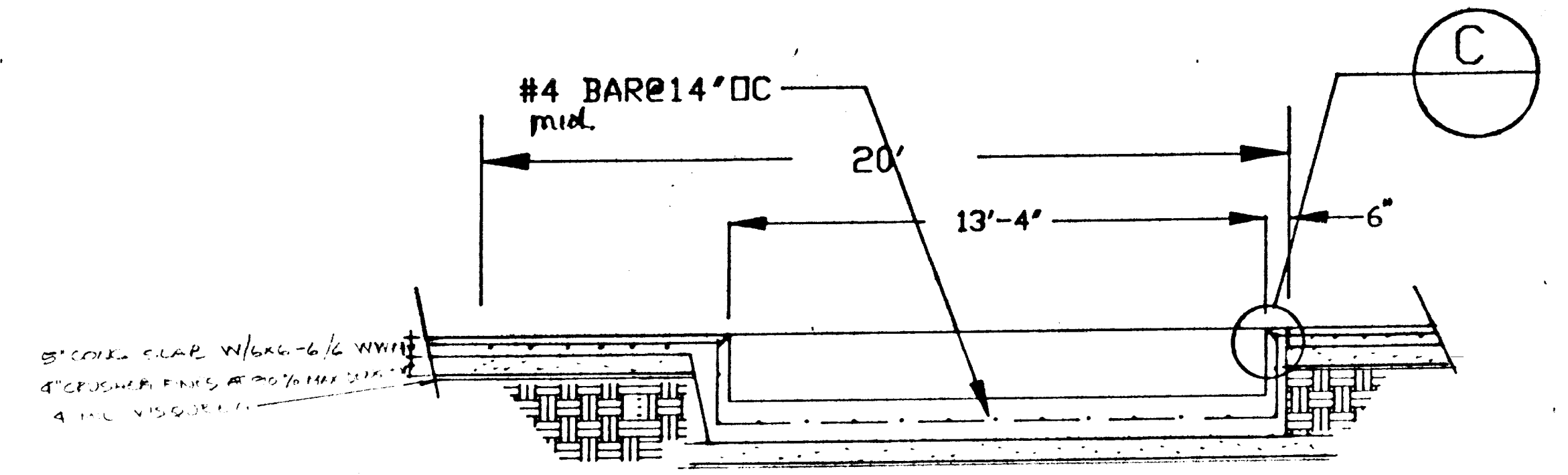




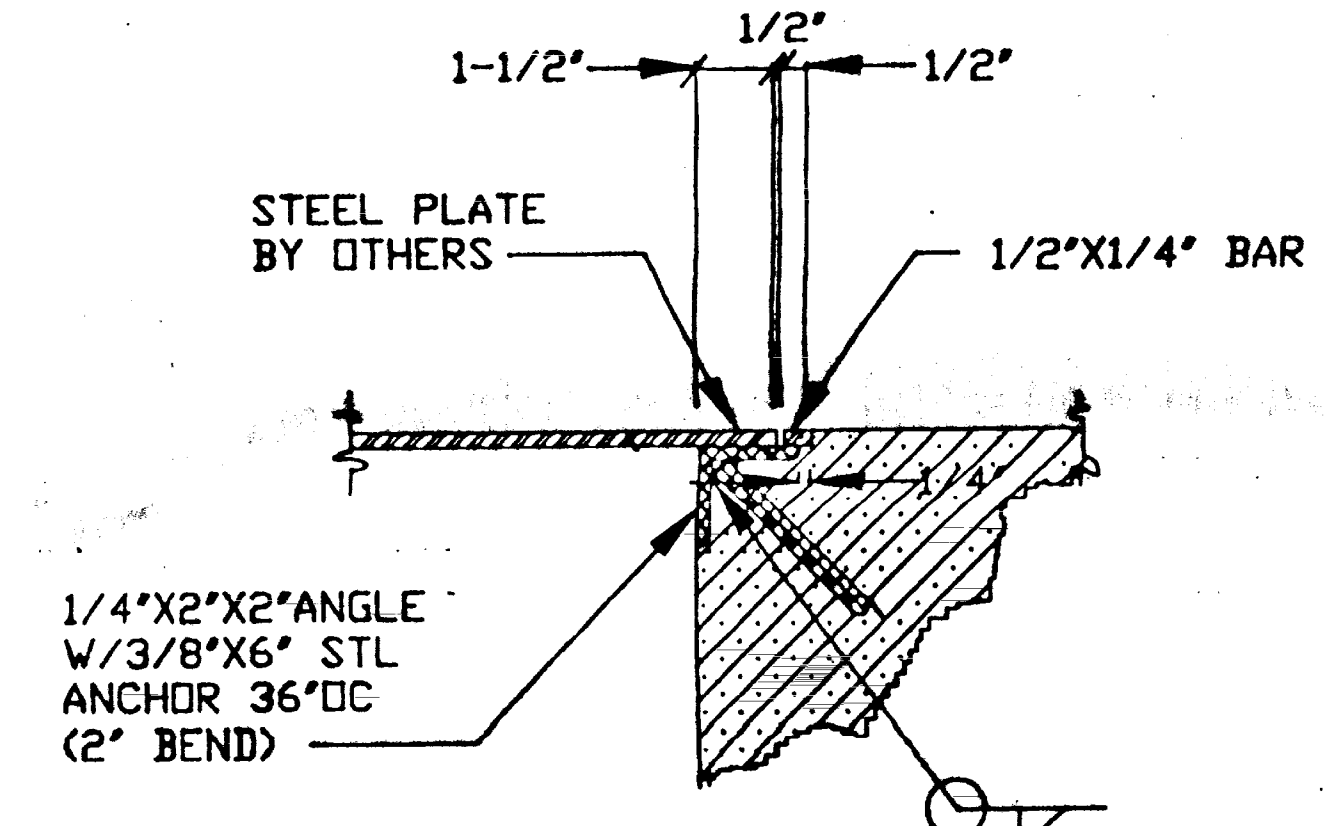
A DETAIL DOWEL WALL  
NTS



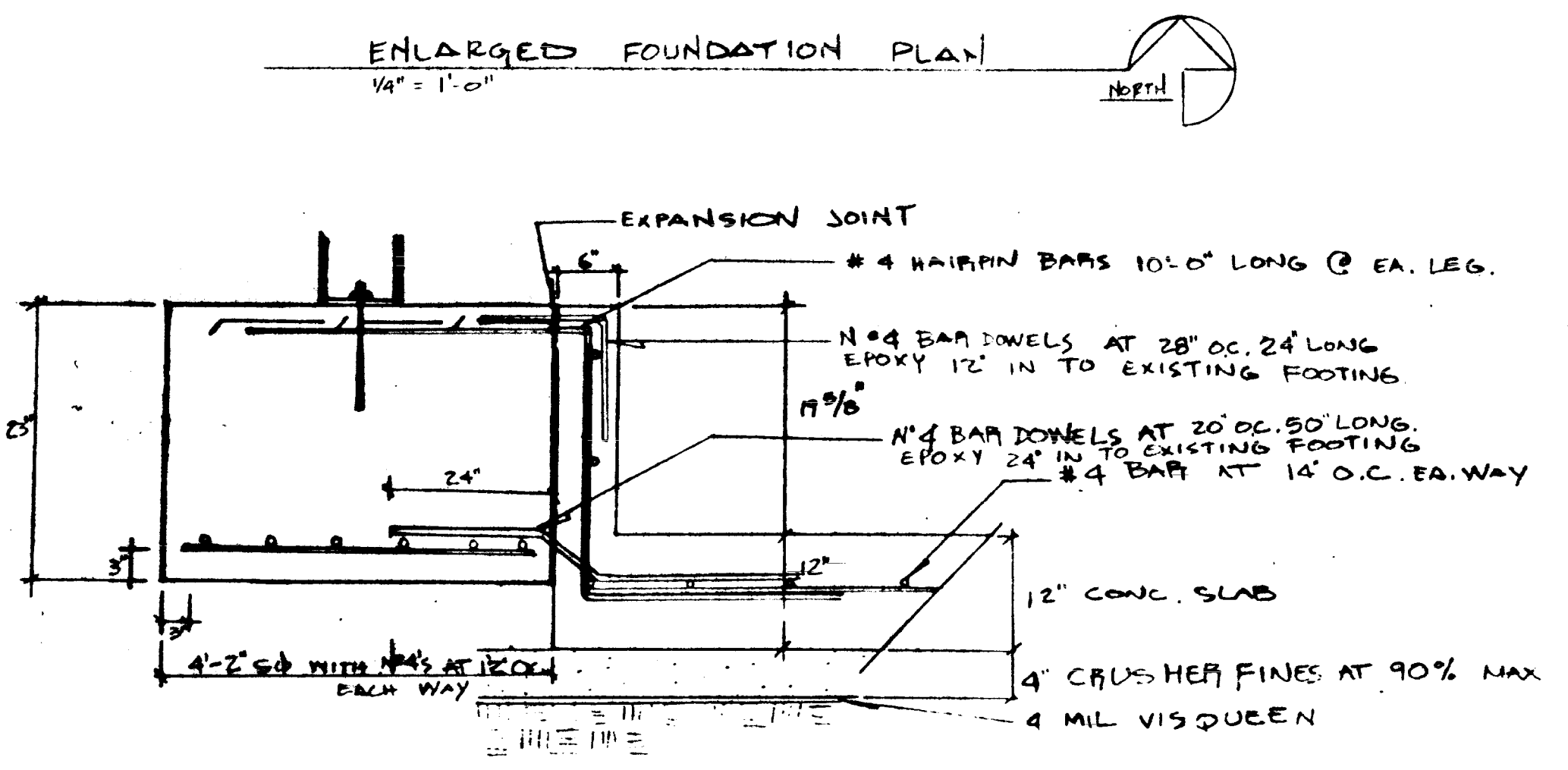
DETAIL SITE



SECTION VIEW FOUNDATION  
NTS



ANCHOR DETAIL  
NTS



D DETAIL FOOTING & HAIRPIN

NOTES

- ① CONCRETE SHALL BE MINIMUM 3000 PSI AT 28 DAY BREAK TEST
- ② TOE OF CONCRETE WALLS TO BE FLUSH WITH FLOOR OF SLAB AND HAVE LIGHT BROOM FINISH.
- ③ TROWEL FINISH OF SLAB WALLS.
- ④ ALL HOLD DOWNS SHALL BE HELD IN TRUE ALIGNMENT, ELEVATION, PLUMB WHILE POUR IS IN PROGRESS.
- ⑤ EXPANSION JOINT TO BE 3/4" ASPHALT IMPREGNATED WITH CELOTEX.
- ⑥ REINFORCING STEEL SHALL BE INTERMEDIATE GRADE BILLET STEEL ASTM DESIGNATION A615 GRADE 4.
- ⑦ ALL DIMENSIONS UNLESS OTHERWISE SPECIFIED



<b>DUGGINS CONSTRUCTION INC.</b>		
266 CEDAR ST. EL CENTRO, CA 92243 619-359-5600		
GENERAL DYNAMICS - CONVAIR DIVISION IMPERIAL, CA		
SCALE NOTED	APPROVED BY	DRAWN BY D.G.
DATE	REVISION	
ENLARGED FOUNDATION PLAN		
DRAWING NUMBER		4 B OF 18

**BUILDING INSPECTION DIVISION**  
COUNTY OF IMPERIAL CALIFORNIA

**APPROVED**

ZONE..... PERMIT NO. *29289*

SETBACK: FRONT..... REAR..... SIDE.....

APPROVED BY: *[Signature]* DATE *11-17-89*

THIS IS NOT A BUILDING PERMIT

**RECEIVED**

NOV 2 1989

IMPERIAL COUNTY  
BUILDING DIVISION

**OFFICE COPY**

**EXHIBIT C**  
**(See Next Page)**



# Imperial County

## Building Inspection Division

\*

## Planning Department

# PERMIT



# APPLICATION

<input checked="" type="checkbox"/> BUILDING	<input type="checkbox"/> NEW RESIDENTIAL	<input type="checkbox"/> REMODEL RESIDENTIAL
<input checked="" type="checkbox"/> ELECTRICAL	<input type="checkbox"/> NEW COMMERCIAL	<input type="checkbox"/> REMODEL COMMERCIAL
<input checked="" type="checkbox"/> PLUMBING	<input checked="" type="checkbox"/> NEW INDUSTRIAL	<input type="checkbox"/> REMODEL INDUSTRIAL
<input checked="" type="checkbox"/> MECHANICAL	<input type="checkbox"/> NEW POOL	
<input checked="" type="checkbox"/> GRADING	<input type="checkbox"/> NEW SIGN	
<input type="checkbox"/> DEMOLITION	<input type="checkbox"/> MOBILE HOME	

APPLICANT MUST COMPLETE ALL NUMBERED (black) SPACES ----- Please type or print ---- thank you!!!

PROPERTY OWNERS NAME <b>1. COUNTY OF IMPERIAL</b>	MAIL ADDRESS	ZIP <b>92243</b>	PHONE <b>339-4236</b>
PROPERTY ADDRESS <b>2. 450 W. ATEN RD. IMPERIAL, CA</b>			
ARCHITECT or ENGINEER <b>3. LYON ENGINEERS</b>	ADDRESS <b>1122 STATE ST. EL CENTRO, CA</b>	CAL. LICENSE NO. <b>27137</b>	PHONE <b>353-8110</b>
CONTRACTOR <b>4. DUGGINS CONST., INC.</b>	ADDRESS <b>265 CEDAR AVE. EL CENTRO</b>	CAL. LICENSE NO. <b>290934</b>	PHONE <b>352-5600</b>
ASSESSORS PARCEL NO. <b>5. 043-50-15</b>	LEGAL DESCRIPTION <b>E. 80 ACRES OF TRACT 101 15 S.R. 13-14 E.S.B.M.</b>		
STATE or DESCRIBE INTENDED USE of BUILDING or PROJECT <b>6. MFG. FACILITY FOR MECHANICAL SUBASSEMBLY OF AIR CRAFT &amp; CRUISE MISSEE PARTS</b>			
DESCRIBE clearly the PROPOSED WORK <b>7. 290' X 225' METAL BUILDING WITH PAVED DRIVEWAYS AND PARKING AREAS, LOADING DOCK WITH TWO (2) TRUCK WELLS</b>			
ARE TWO (2) SETS OF PLANS INCLUDED	<input checked="" type="checkbox"/> YES <input type="checkbox"/> NO		
ARE TWO COPIES OF A SITE PLAN INCLUDED	<input type="checkbox"/> YES <input type="checkbox"/> NO		
HAVE YOU READ AND FOLLOWED THE INSTRUCTION ON THE BACK SIDE	<input type="checkbox"/> YES <input type="checkbox"/> NO		

— NOTICE — NOTICE — NOTICE — NOTICE —

**10a. CONTRACTORS' DECLARATION**

I HEREBY AFFIRM THAT I AM LICENSED UNDER THE PROVISIONS OF CHAPTER 9 DIVISION 3 OF THE BUSINESS AND PROFESSIONS CODE, AND MY LICENSE IS IN FULL FORCE AND EFFECT.

SIGNATURE <i>Ray A. Duggins</i>	LIC. CLASS <b>B-1</b>	DATE <b>8-25-89</b>
------------------------------------	--------------------------	------------------------

**10b. OWNER-BUILDER DECLARATION**

I HEREBY AFFIRM THAT I AM EXEMPT FROM THE CONTRACTOR'S LICENSE LAW FOR THE FOLLOWING REASON:  
I, AS OWNER OF THE PROPERTY, OR MY EMPLOYEES WITH WAGES AS THEIR SOLE COMPENSATION, WILL DO THE WORK, AND THE STRUCTURE IS NOT INTENDED OR OFFERED FOR SALE ((Sec. 7044, Business and Professions Code: "The Contractors License Law does not apply to an owner of property who builds or improves thereon, and who does such work himself, or through his own employees, provided that such improvements are not intended or offered for sale. If however the building or improvement is sold within one year from the date of completion, the owner will have the burden of proving that he did not build or improve the for purpose of sale.))

I, AS OWNER OF THE PROPERTY, AM EXCLUSIVELY CONTRACTING WITH LICENSED CONTRACTORS TO CONSTRUCT THE PROJECT. ((See Sec. 7044, Business and Professions Code.))

SIGNATURE	DATE
-----------	------

**10c. EXEMPTION DECLARATION**

I CERTIFY THAT IN THE PERFORMANCE OF THE WORK FOR WHICH THIS PERMIT IS ISSUED, I SHALL NOT EMPLOY ANYONE IN ANY MANNER SO AS TO BECOME SUBJECT TO THE WORKERS' COMPENSATION INSURANCE LAWS OF CALIFORNIA. Notice to Applicant; If after making this Certificate of Exemption, you should become subject to the Workers' Compensation Provisions of the Labor Code, you must forthwith comply with such provisions or this permit shall be deemed revoked.

SIGNATURE	DATE
-----------	------

I HEREBY CERTIFY THAT I HAVE READ AND EXAMINED THIS APPLICATION AND ALL INFORMATION IS TRUE AND CORRECT. ALL LAWS AND ORDINANCES / CODES WILL BE COMPLIED WITH WHETHER SPECIFIED HEREIN OR NOT.

ZONE	VALUATION
OCCUPANCY GROUP	AREA
TYPE OF CONST.	

*1162,675.00*  
*Ed. Shack 817*  
*VN-SPRINKLED TRK. WELL 24007*

QTY.	ITEM	FEE
1	ELECTRICAL PERMIT	1000
1	MAIN SERVICE (1600 amp.)	480.00
	BREAKER OR OTHER CONTROL BOX	
	SWITCHES, LIGHTS, OUTLETS, ETC.	
1	MOTORS, TRANSFORMERS, ETC.	30.00
	OTHER	
	<b>SUB. TOTAL</b>	<b>\$ 530.00</b>
1	PLUMBING PERMIT	10.00
42	EACH FIXTURE OR TRAP	63.00
1	EACH SEWER CONNECTION	10.00
1	EACH WATER HEATER	5.00
1	WATER PIPING (1-TO-4 outlets)	5.00
1	GAS PIPING (1-TO-4 outlets)	5.00
43	OTHER WATER OUTLETS	63.00
	<b>SUB. TOTAL</b>	<b>\$ 156.00</b>
1	MECHANICAL PERMIT	15.00
44	A/C WITH DUCT (BTU)	396.00
3	VENTILATION FAN	9.00
2	RANGE HOOD	5.00
44	HEATING UNIT	396.00
	OTHER	
	<b>SUB. TOTAL</b>	<b>\$ 816.00</b>

SIGNATURE OF OWNER	SIGNATURE OF CONTRACTOR
<i>[Signature]</i>	<i>Ray A. Duggins</i>
date	date
	<b>8-25-89</b>

## APPROVALS

WORKERS' COMP. INS. <i>750-89</i> Energy Cert. <i>18A+B</i> <input type="checkbox"/> yes <input type="checkbox"/> no <input type="checkbox"/> n.a. SPECIAL PERMIT	ZONING <i>750-89</i> Date E.H.S. (sanitation) <i>9-19-89</i> Date PUBLIC WORKS Date FIRE DEPT. Date A.P.C.D. <i>9-19-89</i> Date
--	--

APPLICATION ACCEPTED BY *JEM* DATE *8-28-89*

APPROVED BY *[Signature]* DATE

S. M. I.	263.30
BUILDING PERMIT	5051.50
PLAN CHECK	3283.48
ELECTRICAL	530.00
PLUMBING	156.00
MECHANICAL	816.00
DEMOLITION	5
GRADING	602.00
OTHER GRADING	30.00
Z.A. SPRINKLER	572.00
PLAN CHECK	371.80

FEE \$ 11,676.08

CONVAIR / DUGGINS CONST.

730-89 ✓

Permit Number

CITY OF IMPERIAL  
APPLICATION TO PERFORM WORK

\*\*\*\*\*

Application for (circle):

WATER

SEWER

FENCE

STRUCTURE

CURB/GUTTER

SIDEWALK

DRIVEWAY APPROACH

OTHER \_\_\_\_\_

\*\*\*\*\*

Any person desiring to perform any kind of work shall first make application to the City of Imperial for issuance of a permit and shall furnish all information which may be required.

As a condition to the issuance of such permit, a good and sufficient bond may be required, either cash or surety, to be posted to guarantee that the work shall be properly done and to reimburse the City of Imperial for any expense for damage which it may suffer by reason of the failure of such person to properly perform such work in accordance with the terms and conditions of the permit.

PROPERTY OWNER County of Imperial PHONE 339-4234

APPLICANT Delva Sigala, F. Duggins Const., Inc. PHONE 352-5600

CONTRACTOR'S NAME & ADDRESS Duggins Construction, Inc. PHONE 352-5600

210 Cedar Ave El Centro, Ca 92243

LOCATION OF WORK 450 W. Allen Rd, Imperial, Ca

WORK TO BE DONE:  
(Attach Sketch) New Bldg. Facility - as per attached blueprints

DATE 9-6-89 Signature of Applicant Duggins Const., Inc. by Delva Sigala

\*\*\*\*\*

ZONING I-2 SETBACKS: Front  Rear  Side  Side

DEPOSITS: \_\_\_\_\_ CAPACITY CHARGES: \_\_\_\_\_ BOND REQUIRED N/A

WATER \* \_\_\_\_\_ \* \_\_\_\_\_

SEWER \* \_\_\_\_\_ \* \_\_\_\_\_

ADDITIONAL COMMENTS: \*Water & Sewer charges to be determined per in to installation

APPROVED  DENIED \_\_\_\_\_ DATE Sept. 6, 1989

APPROVED BY Fay J. Miller TITLE Director of P.W./Planning



**IMPERIAL COUNTY  
SCHOOL DISTRICT CERTIFICATION OF COMPLIANCE  
SCHOOL FACILITY FEES**

**PART I** To Be Completed by Department Issuing Building Permits

Permitting Agency Co. of IMPERIAL - Bldg. Dept.  
 Contact Person MICK MORRISON Title ASSIST. DIR  
 Plan Check # NA Date 9-14-89 Phone # 339-4236  
 Assessor Parcel No(s). 043-50-15-01  
 Project Owner's Name ED CONVAIR  
 Project Address 450 W. ATEN RD. IMPERIAL  
 Project Description (Include no. of Dwelling Units and drawing(s) of covered and enclosed areas to be furnished to school district.) Construct Manufacturing Facility  
 Building Type (Circle One): Residential Commercial/Industrial  
 Square Feet of Covered or Enclosed Building Area for Project 65,250 #  
 (On additions to existing buildings include only new construction square footage.)

**PART II** To Be Completed by School District(s)

School district requirements for the above project have been satisfied pursuant to (Circle One):

Gov. Code 53080 (Ab 2926)	Gov. Code 65970 (SB 201)	Agreement Existing Before 1/1/87	Not subject fee requirements
---------------------------------	--------------------------------	-------------------------------------	---------------------------------

A signature is required from each district of attendance affected by the construction of a building.

**A. School District Elementary:**

Seal/Stamp

Name of District	Phone No.	Date
Signature	Title	

**B. School District Secondary:**

Seal/Stamp

Name of District	Phone No.	Date
Signature	Title	

**C. School District Unified:**

Seal/Stamp

<u>Imperial Unified</u> Name of District	<u>355-2433</u> Phone No.	<u>9-15-89</u> Date
<u>Patti Kiser</u> Signature	<u>Admin. Asst.</u> Title	

FOOTING CALCS (SPECIAL)

REF: MANUFACTURERS SPECIFICATIONS - SELECT PRODUCERS TECH DATA  
 REINFORCED CONCRETE FUNDAMENTALS - 4th EDITION - FERGUSON  
 AMERICAN INST. FOR STEEL CONSTRUCTION - 6th EDITION

CHECK EPOXY

$P_{ALLOW} = \text{TENSILE STRENGTH OF GROUT} \times \text{CIRCUMFERENCE OF PER} \times \text{LENGTH OF EMBEDMENT}$   
 $= 8000 \text{ psi} \times 1.67 \text{ IN} \times 24 \text{ IN} = 301.1 \text{ K} \geq 3.3 \text{ K}$

**∴ EPOXY IS MORE THAN ADEQUATE**

$P_{ACTUAL} = \frac{\text{LOAD APPLIED}}{\text{NO. RESISTING BARS}} = \frac{6.6 \text{ K}}{2} = 3.3 \text{ K}$

NOTE: 8000 psi TENSILE STRENGTH PER MANUFACTURER SPECS FOR G

CHECK CONCRETE PULL-OUT : NOTE: CONCRETE FAILS AT 45° TO APPLIED LOAD IN SHEAR

$P_{ALLOW} = \text{CONICAL SHEAR FAILURE AREA} \times \text{TENSILE STRENGTH OF CONCRETE} \times 1/2$   
 $= 2 \pi R \times L \times T_c / 2 = 2 \times 3.14 \times 24 \text{ IN} \times 24 \text{ IN} \times 75 \text{ PSI} / 2$  NOTE: 75 PSI ≤ 10% of  $f_c = 2000 \text{ PSI}$  → P.C.  
 $= 1356 \text{ K} \geq 3.3 \text{ K}$

**∴ CONCRETE IS MORE THAN ADEQUATE AND WILL NOT FAIL**

∴ VERY CONSERVATIVE

$P_{ACTUAL} = 3.3 \text{ K}$  ← FROM ABOVE

CHECK STEEL FOR YIELD

$P_{ALLOW} = \text{ALLOWABLE TENSILE STRENGTH} \times \text{CROSS SECTIONAL AREA}$   
 $= 20000 \text{ psi} \times 0.1963 \text{ IN}^2 = 3916 \text{ LBS} \geq 3.3 \text{ K}$

NOTE:  $20 \text{ IN} \times 0.16 \times 36 \text{ K} = 21.6 \text{ K}$  ← L.S.  
 ∴ CONSERVATIVE

$P_{ACTUAL} = 3.3 \text{ K}$  ← FROM ABOVE

**∴ STEEL IS ADEQUATE AND WILL NOT FAIL**

**∴ USE A MIN OF 2 - NO. 4 BARS EPOXY 24" INTO EXISTING FOOTING, 60" LONG, 28" O.C. MIN**


**∴ THE FOOTING AND HAIRPINS AS DESIGNED ARE MORE THAN ADEQUATE**

**RECEIVED**

NOV 14 1989

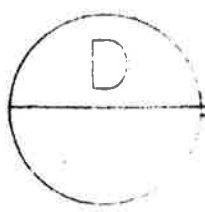
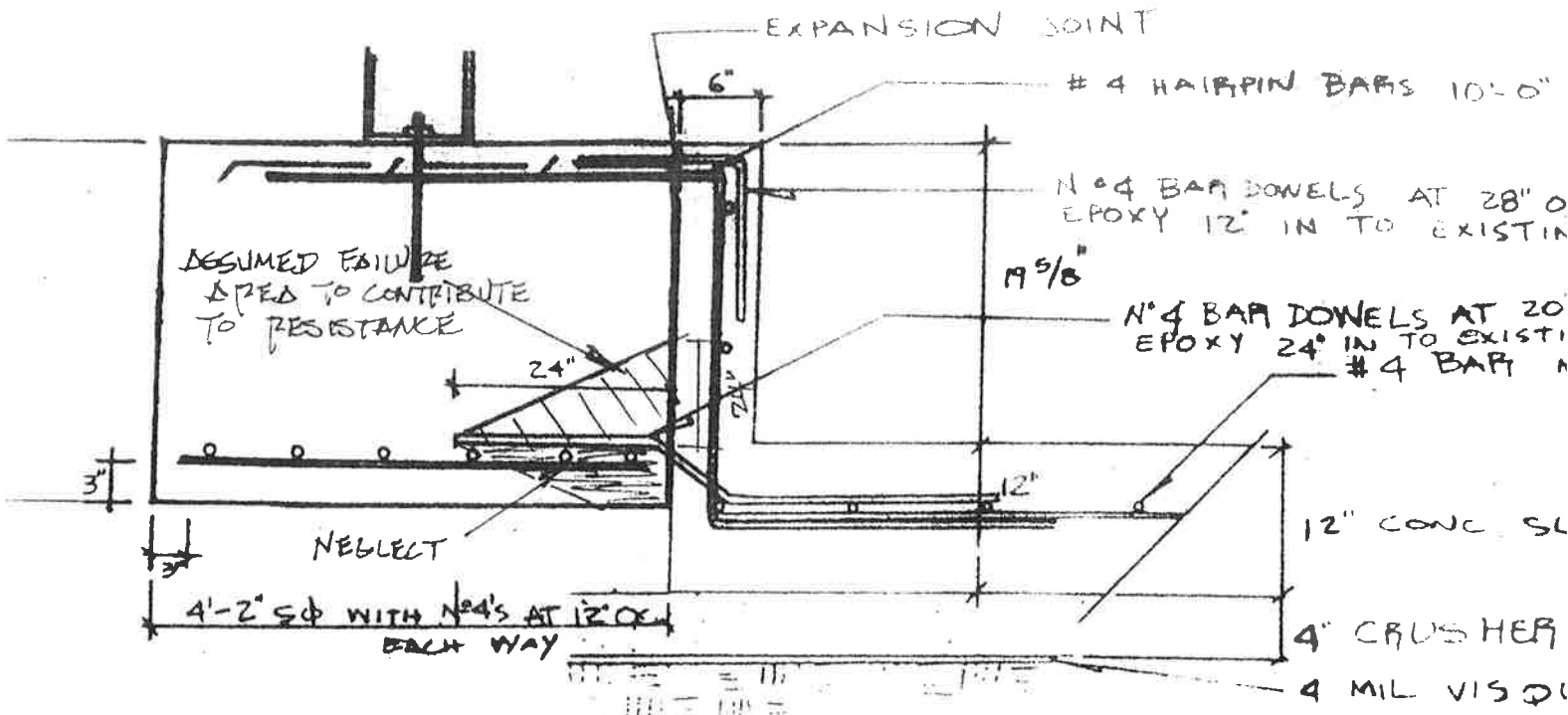
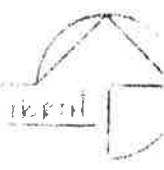
IMPERIAL COUNTY BUILDING DIVISION



<b>Lyon Engineers, Inc.</b> CIVIL ENGINEERING  1122 STATE STREET EL CENTRO, CALIFORNIA 92243 619-353-8110	SHEET 1		
	OF		
	IMPERIAL COUNTY	DATE 11/14/89	BY
	CLIENT DUBOIS	JOB NO 89236	1

ENLARGED FOUNDATION PLAN

1/4" = 1'-0"



DETAIL FOOTING & HAIRPINS

MONORAIL 32'-0" SPAN - DECEASED LENGTH 16'-0"

1000<sup>lb</sup> CAPACITY - 1 PEIZ BEAM - SEEMS LOW  
 100<sup>lb</sup> WEIGHT HOIST  
 12<sup>lb</sup>/FT WEIGHT BEAM

$R_A = 742^{\#} = P_B$  OK

$R_B = 1842$   $P_B = 355$

STRENGTH

MOMENT =  $4784^{\#-ft}$   $L = 16'$

MOMENT =  $8800^{\#-ft}$   $L = 8'$

OK  $M_p = 7000^{\#-ft} \geq 4784^{\#-ft}$  OK

OK  $M_p = 13500^{\#-ft} \geq 8800^{\#-ft}$

TRY W6x12

TRY W6x12

SHEAR

$F_{V_{allow}} = 0.4 F_y = 0.4 \times 36000 = 14400$

$F_v = \frac{V}{d \cdot t_w}$

$V_{max} = 742^{\#}$

$V_{max} = 1842^{\#}$

$F_v = \frac{742^{\#}}{6.03 \times 0.23} = 535 \text{ psi} \leq 14400$

$F_v = \frac{1842^{\#}}{6.03 \times 0.23} = 1326 \text{ psi} \leq 14400$

DEFLECTION - MAXIMUM

$\Delta \leq \frac{5wL^4}{384EI} + \frac{PL^3}{48EI}$

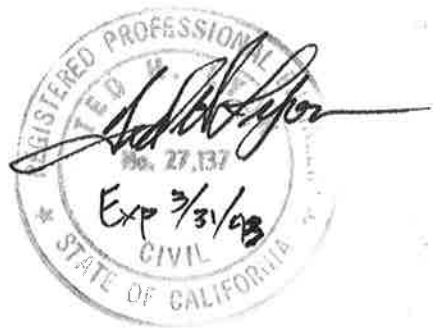
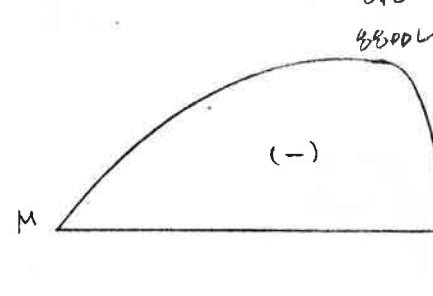
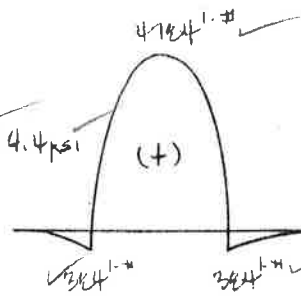
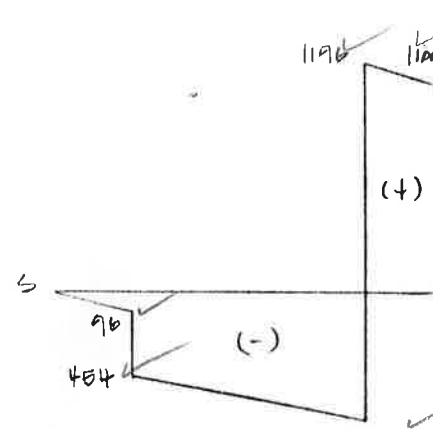
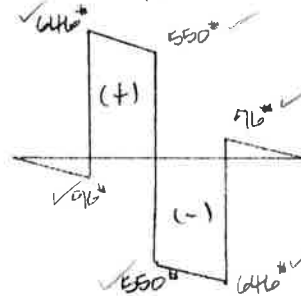
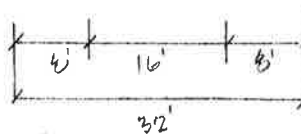
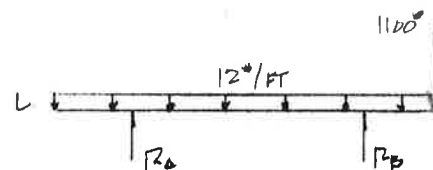
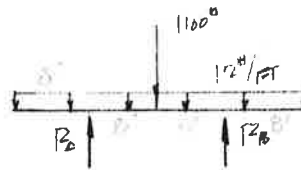
CONCENTRATED

$\leq \frac{5 \times 12^{\#}/ft \times 16^4}{384 \times 29 \times 10^6 \text{ psi} \times 22.1 \text{ in}^4} + \frac{1100^{\#} \times 16^3}{48 \times 29 \times 10^6 \text{ psi} \times 22.1 \text{ in}^4} = 0.03" + 0.25" = 0.28"$

$\Delta_{allow} = \frac{l}{360} = \frac{16' \times 12^{\#}/ft}{360} = 0.53" \geq 0.28" \quad \text{OK}$

USE W6x12 BEAM FOR 32'-0" SPAN

OK



**Lyon Engineers, Inc.**  
 CIVIL ENGINEERING  
 1122 STATE STREET  
 EL CENTRO, CALIFORNIA 92243  
 619-353-8110

MONORAIL GATES		SHEET 1
GENERAL DYNAMICS		
IMPERIAL, CALIFORNIA	DATE 1/3/90	BY G.D.
CLIENT DUBBINS CONST.	JOB No 90001	



MONORAIL 43'-9" SPAN - UNBRACED LENGTH 21'-1"

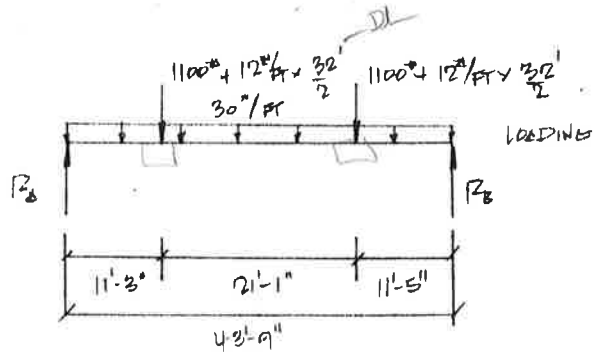
1000# CAPACITY - 2 PER BEAM  
 100# WEIGHT HOIST - 2 PER BEAM  
 12#/FT BEAM 1/2 - 30#/FT BEAM

$$EM_B = 0 = 1292 \times 11.3 + 30 \times 43.9 \times \frac{43.9}{2} + 1292 \times 32.4 - R_B \times 43.9$$

$$R_B = 1943.2$$

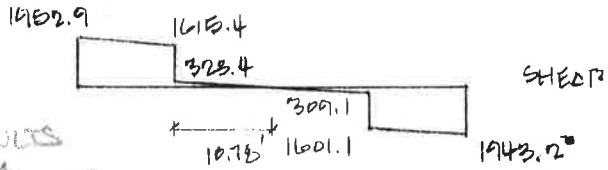
$$EM_A = 0 = 1292 \times 11.5 + 30 \times 43.9 \times \frac{43.9}{2} + 1292 \times 32.6 - R_A \times 43.9$$

$$R_A = 1952.9$$



$$\frac{21'-1''}{632.5} = \frac{x}{323.4}$$

$$x = 10.75$$



STRENGTH

MOMENT = 21814.8 <sup>LOW</sup>

$$M_R = 27000 \geq 21814.8 \rightarrow \text{SHOULD REMAIN SAME} \quad \therefore \text{OK}$$

TRY W10x26

STEEL

$$F_{allow} = 0.4 F_y = 0.4 \times 36 \text{ ksi} = 14.4 \text{ ksi}$$

$$R = \frac{V}{d/t_w}$$

$$V_{allow} = 1952.9 \text{ (LOW)}$$

$$R = \frac{1952.9}{12.34 \times 0.26} = 608.7 \text{ psi} \leq 14.4 \text{ ksi} \quad \therefore \text{OK}$$

WHERE DID THIS COME FROM? CHECK AGAIN

DEFLECTION

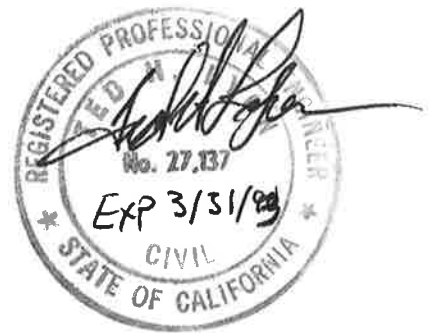
$$\Delta = \frac{5wL^4}{384EI} + \frac{P_a (3L^2 - 4a^2)}{24EI} = \frac{5 \times 30 \times 43.9^4 \times 1728 \text{ in}^3/\text{ft}^3}{384 \times 29 \times 10^6 \text{ psi} \times 238 \text{ in}^4} + \frac{1292 \times 11.5 \times 12 \times \left[ (3 \times 43.9^2) - 4 \times 11.5^2 \right]}{24 \times 29 \times 10^6 \text{ psi} \times 238 \text{ in}^4}$$


$$= 0.36 + 0.80 = 1.16 \text{ in}$$

$$\Delta_{allow} = \frac{L}{300} = \frac{43.9 \times 12}{300} = 1.45 \text{ in} \geq 1.16 \text{ in} \quad \therefore \text{OK}$$

USE W12x30 BEAM FOR 43'-9" SPAN

CALCS ARE INCORRECT BUT SIZE SHOULD BE ADEQUATE



<b>Lyon Engineers, Inc.</b> CIVIL ENGINEERING  1122 STATE STREET EL CENTRO, CALIFORNIA 92243 619-353-8110	MONORAIL CALCS	SHEET 2 OF
	GENERAL DYNAMICS	
IMPERIAL CALIFORNIA	DATE 1/3/90	BY E.D.
CLIENT DUBBINS CONST.	JOB NO. 07001	

COLUMNS

SEISMIC ANALYSIS

$V = \Sigma I KCSW$

$Z = 1 \quad K = 2^{1.1} \quad S = 1.2$

$I = 1 \quad C = 0.12 \quad C_s = 0.14 \text{ MAX}$

$T_L = 1000 \sqrt{2} + 100 \sqrt{2} + 12 \sqrt{11} + 32 \sqrt{2} + 30 \sqrt{11} + 43 \sqrt{11} + 2 + 65 \sqrt{11} + \frac{14}{2} \times 4 = 7413$

$V = 7413 \times \frac{1}{4} = 1853.3$

$M = 1853.3 \times 14 = 25945.5$

$K = 1 \times 14 = 14$

$P_{EFF} = P + M_y m_U = 1957 + 25945.5 \times 2.2 \times 3 = 173197.3$  — TRY W 12x40

$P_{EFF} = 1957 + 25945.5 \times 2.0 \times 3.17 = 197586$  — TRY W 12x45

$P_{EFF} = 1957 + 25945.5 \times 2.0 \times 4.10 = 214710$  — USE W 12x50

∴ USE W 12x50 COLUMN FOR  
14'-0" HEIGHT

OK

BASE PLATE

$A_1 = \frac{1}{\Delta_2} \times \frac{P}{0.35 f_c} = \frac{1}{613.5 \text{ IN}^2} \times \frac{214710}{0.35 \times 2000 \text{ psi}} = 153.4 \text{ IN}^2$

$\Delta_1 = \frac{P}{0.7 f_c} = \frac{214710}{0.7 \times 2000 \text{ psi}} = 153.4 \text{ IN}^2$

$\Delta_2 = \frac{P}{0.175 f_c} = \frac{214710}{0.175 \times 2000 \text{ psi}} = 613.5 \text{ IN}^2$

$N = \sqrt{\Delta_1 + \Delta_2} = \sqrt{153.4 \text{ IN}^2 + 0.5(0.95d - 0.60b)} = \sqrt{153.4 \text{ IN}^2 + 0.5(0.95 \times 12.19 - 0.60 \times 6.0)} = 14.95 \text{ IN}$

$B = \Delta_1 / N = 153.4 \text{ IN}^2 / 14.95 \text{ IN} = 10.26 \text{ IN}$

$f_p = P / B \times N = 214710 / (10.26 \times 14.95) = 1399.8 \text{ psi}$

$m = (N - 0.95d) / 2 = (14.95 - 0.95 \times 12.19) / 2 = 1.62$

$n = (B - 0.60b) / 2 = (10.26 - 0.60 \times 6.0) / 2 = 1.89$

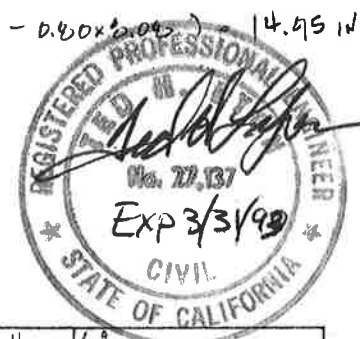
$n' = 3.61$


$t_p = n' \sqrt{\frac{f_p}{0.25 f_y}} = 3.61 \sqrt{\frac{1399.8 \text{ psi}}{0.25 \times 36000 \text{ psi}}} = 1.42 \approx 1.5$

USE BUT OK

∴ USE 15" x 11" x 1/2" BASE PLATE

OK



<b>Lyon Engineers, Inc.</b> CIVIL ENGINEERING  1122 STATE STREET EL CENTRO, CALIFORNIA 92243 619-353-8110	MONORAIL CALCS	SHEET 3
	GENERAL DYNAMICS	
IMPERIAL, CALIFORNIA	DATE 1/4/00	BY E.O.
CLIENT FALGOUTS CONST.	JOB NO 90001	

ANCHOR BOLTS

W12x50 COLUMN  $d = 12.19" \quad l_f = 8.08" \quad t_f = 0.64"$

AXIAL LOAD = 1957  $\checkmark$  OK

MOMENT = 25945.5  $\checkmark$  OK

A36 STEEL  $F_u = 58 \text{ ksi} \quad F_y = 36 \text{ ksi}$  ASSUMED

$e = \frac{12 \times 25945.5}{1.96 \text{ k}} = 158.82 \text{ IN}$

$E M_o = 1.96 \text{ k} (158.82 - 5.77) - 13.12 \text{ T} = 0$

$T = 22.86 \text{ k} \quad \text{OK}$

ANCHOR BOLT AREA REQ =  $\frac{22.86 \text{ k}}{36 \text{ ksi}} \cdot 1.64 \text{ in}^2 = 1.01 \text{ IN}^2$

USE 4-7/8"  $\phi$  A307 ANCHOR BOLTS WITH 19" EMBEDMENT EACH  $\checkmark$

WELDS

$P = 22.86 \text{ k} \quad \checkmark$

MAX WELD SIZE =  $0.64 - 1/16" = 0.58"$

MIN WELD SIZE = 0.25"

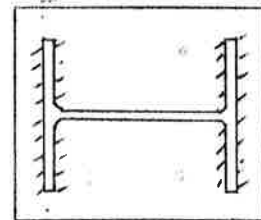
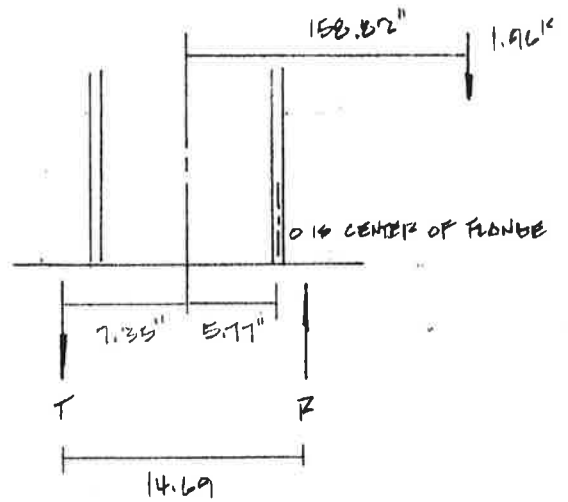
USE 3/8" WELD OK

$t_e = 0.707 \times 3/8" = 0.2651" \quad \checkmark$

CAPACITY OF WELD =  $0.2651" \times 0.30 \times 70 = 5.57 \text{ k/IN}$

LENGTH REQ =  $\frac{22.86 \text{ k}}{5.57 \text{ k/IN}} = 4.10 \text{ IN} \leq 16" \quad \therefore \text{OK}$

USE 3/8" FILLET WELDS AS SHOWN  $\checkmark$



**Lyon Engineers, Inc.**

CIVIL ENGINEERING



1122 STATE STREET  
EL CENTRO, CALIFORNIA 92243  
619-353-8110

MONORAIL COLS		SHEET 4
GENERAL DYNAMICS		
IMPERIAL, CALIFORNIA	DATE 4/90	BY E.D.
CLIENT PUGGINE CONST.	JOB NO 90001	

# FOOTING

$$TL = 1952.9\# \leftarrow \text{LOW}$$

$$\text{SOIL BEARING CAPACITY} = 1000 \text{ PSF} \left( \frac{24}{12} / \text{FT} \times 144 \text{ PCF} \right) = 712 \text{ PSF}$$


$$\text{AREA REQ.} = \frac{1952.9}{712 \text{ PSF}} = 2.74 \text{ SF} \leq 4.00 \text{ SF}$$

∴ USE 2'-0" Sφ AT 24" DEPTH  
WITH N° 4'S AT 12" OC EACH WAY

TOTAL  
LOAD IS  
LOW BUT  
SIZE OF FOOTING  
IS OVERSIZED ANYWAY  
SO DESIGN IS SUFFICIENT



**Lyon Engineers, Inc.**  
CIVIL ENGINEERING



1122 STATE STREET  
EL CENTRO, CALIFORNIA 92243  
619-353-8110

MONORAIL CALS		SHEET 5
GENERAL DYNAMICS		
IMPERIAL CALIFORNIA		DATE 7/22/90 BY O.R.D.
CLIENT DUGGINS CONST.	JOB NO 90001	OF





# SELECT PRODUCTS TECHNICAL DATA



• FOR THE CONSTRUCTION INDUSTRY •

## Select Bond GP-2000 Adhesive Binder—General Purpose

### 1. PRODUCT NAME

SELECT BOND GP-2000 GENERAL PURPOSE EPOXY ADHESIVE BINDER.

### 2. MANUFACTURER/DISTRIBUTOR

SELECT PRODUCTS COMPANY  
1652 W. 11th Street—Unit B  
Upland, California 91786  
Telephone: (714) 985-5771  
Telephone: (714) 985-5069  
Telephone: (714) 985-8366

### 3. PRODUCT DESCRIPTION

GP-2000 is a 100% solids two component liquid epoxy that bonds new concrete to old concrete, wood, metal, etc. to themselves and to each other. GP-2000 also works as a prime coat to these substrates.

GP-2000 is an excellent epoxy adhesive for use as a sand binder, grouting material to produce an epoxy mortar and as a general purpose adhesive.

GP-2000 is ideally suited for injection applications and is available in 10 ounce, pre-measured injecto-kits, as well as in gallon kits, pails and drums.

### 4. TECHNICAL DATA

Mixing Ratio: 2 parts "A" to 1 part "B" by volume.

Epoxy Mortar: Add clean, absolutely dry aggregate and mix to produce the desired mortar or concrete. The prepared mortar must wet the surface to provide proper adhesion. Do not use an excessive amount of aggregate which can produce a dry mortar. The ultimate mixing ratio is 5 parts by weight of aggregate to 1 part of mixed epoxy.

Color: Part "A"—yellow. Part "B"—amber.  
Mixed parts—clear amber.

Consistency: Low viscosity for high penetration into crack and voids.

Pot Life: Working life—20 to 30 minutes @77°F. The addition by weight of 5 parts of sand to 1 part of mixed epoxy will extend the working life by 50%.

Curing Time: 24 hours @77°F.

Shelf Time: Minimum one year in separate unopened containers.

### Typical Physical Properties:

Compressive Strength 7-Days: 13,000 p.s.i. (ASTM-D-695)

Compressive Shear Strength: 3,200 p.s.i.

Heat Deflection Temperature: 191°F. (ASTM-D-648)

Tensile Strength (ASTM-D-638): 8,000 p.s.i.

Effective Shrinkage (ASTM-M-883): passed  
Compressive Modulus of Elasticity: 4.6 x 10 (ASTM-D-695)

Tensile Elongation (ASTM-D-638): 8%

Bond Strength 14-Days: to steel 7,000 p.s.i.

Tack Free Time @ 77°F: 4 hours

Viscosity: 1,200 centipoises

IZOD Impact (Ft. lbs./in. notch): 0.72

### 5. INSTALLATION

Application: By brushing, rolling, pouring, or injecting. Pre-measured 10 ounce injecto-kits are available and are ideal for low volume injection requirements.

Proper surface preparation such as sand-blasting, grinding, wire brushing or other suitable means as required is essential for proper adhesive or coating performance.

Surfaces to receive adhesive or coating must be structurally sound, dry, clean, free of dirt, moisture, loose particles, oil, grease, asphalt, tar, paint, wax, rust, waterproofings, curing and parting compounds, membrane materials, or any other contaminants which might prevent satisfactory bond and epoxy cure.

Thorough mixing of SELECT BOND GP-2000, using mechanical means whenever possible, is essential for satisfactory performance.

Always pay strict attention to ambient, and surface temperature of repair areas. Selection of a "Type II" cold weather SELECT BOND or KOTE may be dictated.

### 6. AVAILABILITY AND COST

SELECT BOND GP-2000 EPOXY is supplied through building material supply houses, dealers, distributors as well as directly from

**SELECT PRODUCTS COMPANY. GP-2000** is a medium priced epoxy. Please contact **SELECT PRODUCTS COMPANY** for prices and marketing information.

**7. PRODUCT WARRANTY**

**SELECT PRODUCTS** are warranted under the following policy: All recommendations, statements, and technical data contained herein are based on tests we believe to be reliable and correct, but accuracy and completeness of said tests are not guaranteed and are not to be construed as a warranty, either expressed or implied. User shall rely on his own information and tests to determine suitability of the product for the intended use and user assumes all risk and liability resulting from his use of the product. Seller's and manufacturer's sole responsibility shall be to replace that portion of the product of this manufacturer which proves to be defective. Neither seller nor manufacturer shall be liable to the buyer or any third person for any injury, loss, or damage directly or indirectly resulting from use of, or inability to use, the product. Recommendations or statements other than those contained in a written agreement signed by an officer of the manufacturer shall not be binding upon the manufacturer or seller.

**8. MAINTENANCE AND PRECAUTIONS**

Apply per recommendations made herein, with equipment suited for use with this compound. For additional information refer to the surface preparation and application sheet.

**WARNING: GP-2000 EPOXY** can cause dermatitis if proper precautions are not taken. Protection to skin and eyes should be provided by the use of gloves and protective creams. If contact occurs with skin, wash thoroughly with soap and water. If it gets in the eyes, flush for ten minutes with water and get immediate medical care. Never try to remove epoxy from skin by using solvent.

**9. TECHNICAL SERVICES**

Technical Assistance can be obtained by contacting:

**SELECT PRODUCTS COMPANY**  
1652 W. 11th Street—Unit B  
Upland, California 91786

**10. MOTOR FREIGHT CLASSIFICATION**

Adhesives, N.O.S., Item 4620 (S-5) Class 60

TO: MICK MORRISON, PLANNING  
FROM: JOHN ARMAS, PUBLIC WORKS

SUBJECT: GENERAL DYNAMICS LIFT

DATE: 1 25 90

MESSAGE

MICK,  
THE ATTACHED CALCULATIONS HAVE BEEN REVIEWED AS REQUESTED. ALTHOUGH THERE ARE SOME MINOR ERRORS THE SHAPE SIZES ARE OF SUFFICIENT SIZE TO SUPPORT A 1000# LOAD ON EACH LIFT. YOU SHOULD REQUEST SOME LIFT SPECS TO INSURE THIS 1000# LIMIT IS NOT EXCEEDED.

PLEASE REPLY TO → SIGNED

*John Armas*

REPLY BY

REPLY

RECEIVED  
JAN 25 1990  
IMPERIAL COUNTY  
BUILDING DIVISION

SIGNED

GBF 4A-73N

INSTRUCTIONS TO SENDER

1 KEEP YELLOW COPY 2 SEND WHITE AND PINK COPIES INTACT

INSTRUCTIONS TO RECEIVER

1 WRITE REPLY 2 DETACH STUB, KEEP PINK COPY, RETURN WHITE COPY TO SENDER

TO: Mick Morrison, Planning  
FROM: John Adams, Public Works

SUBJECT: GENERAL DYNAMIC LIFT

DATE: 1 25 90

MESSAGE

Mick,  
THE ATTACHED CALCULATIONS HAVE BEEN REVIEWED AS REQUESTED. ALTHOUGH THERE ARE SOME MINOR ERRORS THE SHAPE SIZES ARE OF SUFFICIENT SIZE TO SUPPORT A 1000# LOAD ON EACH LIFT. YOU SHOULD REQUEST SOME LIFT SPEC TO INSURE THIS 1000# LIMIT IS NOT EXCEEDED.

PLEASE REPLY TO  SIGNED



REPLY BY

REPLY

SIGNED

DATE

GBF 4A-73N

INSTRUCTIONS TO SENDER

1 KEEP YELLOW COPY 2 SEND WHITE AND PINK COPIES INTACT

INSTRUCTIONS TO RECEIVER

1 WRITE REPLY 2 DETACH STUB KEEP PINK COPY RETURN WHITE COPY TO SENDER

**EXHIBIT D**  
**(See Next Page)**



PERMIT FILING SYSTEM INDEX (PLANNING DEPARTMENT)  
 \*\*\*\*\*

APN #	0435001501	*****	
ADDRESS (street)	AIRPORT ROAD	(number)	1101
AREA	COUNTY AIRPORT	SUP.DIST.	3
NAME (last)	COUNTY AIRPORT	(first)	

Permit information

Minor Sub.	PM#	Date app.		by BRD	COM	DIR
Major Sub.	TR#	Date app.		by BRD	COM	DIR
C. U. P.	#	Date app.		by BRD	COM	DIR
Variance	#	Date app.		by BRD	COM	DIR

Zone Change	MAP#	Date app.		Ordinance #	
G.P.A.	MAP#	Date app.		Ordinance #	
C.E.Q.A. only	EIR	Neg. Dec.	Project		
REC. PLAN	#	Date app.		blm	private
Other	AIRPORT LAND USE COMMISSION				

\*\*\*\*\*  
 CROSS REFERENCE TO OTHER APN'S-----  
 \*\*\*\*\*

X-APN-1  X-APN-2  X-APN-3

\*\*\*\*\*  
 \*\*\*\*\*

(continued on next screen)

MONITORING/TRACKING SYSTEM

\*\*\*\*\*

TERM OF PERMIT  \*\*\*press F1 for explanation\*\*\*

1st. MONIT. DATE INSP. BY (staff)	<input type="text"/>	ACTUAL DATE CHECKED COMPLIANCE	Y/N	(if no state why)
2nd. MONIT. DATE INSP. BY (staff)	<input type="text"/>	ACTUAL DATE CHECKED COMPLIANCE	Y/N	(if no state why)
3RD. MONIT. DATE INSP. BY (staff)	<input type="text"/>	ACTUAL DATE CHECKED COMPLIANCE	Y/N	(if no state why)
4TH. MONIT. DATE INSP. BY (staff)	<input type="text"/>	ACTUAL DATE CHECKED COMPLIANCE	Y/N	(if no state why)
UNSCHEDULED INSPECTION #1 #3	<input type="text"/>	#2 #4	<input type="text"/>	

(continued on next page)

EXPLANATIONS/COMMENTS/VIOLATIONS ETC.  
 AIRPORT LAYOUT PLAN-FEIR

IMPERIAL COUNTY  
INTER-OFFICE MEMO

DATE October 25, 1979

TO: DICK MITCHELL, PLANNING DIRECTOR

FROM: DANIEL B. PAVAO, AIRPORT MANAGER *DSP*

SUBJECT: FINAL EIR, EAST-WEST RUNWAY.

DICK,

Under separate cover are transmitted 50 copies of final EIR on subject project, together with revised Airport Layout Plan.

This Office will proceed with submittal to FAA of project pre-application, ALP drawing and other documents as necessary.

I will await your notice of necessary public hearings and/or Board of Supervisors approval of ALP.

IMPERIAL COUNTY

INTER-OFFICE MEMO

DATE OCTOBER 15, 1979

TO: RICHARD MITCHELL, DIRECTOR, PLANNING DEPARTMENT

FROM: DANIEL B. PAVAO, AIRPORT MANAGER/ CHIEF, REAL PROPERTY *DBP*

SUBJECT: AIRPORT ENVIRONMENT (ALP, MASTER PLAN, ETC.)

It is expected that Hodges & Shutt will supply us with the final E.I.R. on the east-west runway, as well as the revised Airport Layout Plan, within the next 3 or 4 weeks; following which it is my plan to obtain Board of Supervisors adoption of said ALP and proceed with funding application approval, appraisal and acquisition this fiscal year.

I am deeply concerned for protecting the new east-west runway, its approaches and environment, as well as the existing runway 14-32.

In this regard, I will approach FAA for Planning Grant Program funds which I understand will be available in the near future (according to Mike Shutt).

I hope we can develop an airport master plan which will prevent the sort of incompatible development near the new runway (8-26) which has impacted runway 14-32.

I welcome any suggestions or information you can furnish me in this regard.

**HODGES & SHUTT**  
AVIATION PLANNING SERVICES



2310 Airport Boulevard • Santa Rosa, California 95401 • (707) 526-5010

September 28, 1979

Dick: Sorry this did not get mailed last Friday.

Mike

RECEIVED

OCT 5 1979

IMPERIAL COUNTY  
PLANNING DEPARTMENT

Mr. Daniel B. Pavao  
Airport Manager  
1101 Airport Road  
Imperial, California 92251

Dear Dan:

As we discussed yesterday, I would like to elaborate further on a couple of matters concerning the Airport.

Final Environmental Report Printing.

As of this date, we have still not received comments on the report from the FAA. They advise us that we can expect them sometime late next week. When these are received, we will need about one week to revise the report and print the necessary copies. Based upon discussions with your office, we now understand that the County will require fifty (50) copies of the report for internal use. We have also been informed by the FAA that they will need sixty (60) copies of this document in order to comply with their new procedural guidelines. Our contract with the County called for printing 50 copies of the final report which was adequate for County and Federal needs last winter. Our printer advises us that it would be about \$500.00 to provide the extra reports. Since this is above and beyond our agreement with the County, please advise us how you would like to handle the matter. You should be aware that printing costs for environmental processing are ADAP reimbursable expenses and approximately 80% of the cost will be paid by the FAA at the time of your first grant.

County-Wide Environs Study.

As you, Dick Mitchell, and I have discussed in the past, the County is definitely in need of land-use planning around all the airports in the County. I discussed the possibility of conducting an FAA-funded study with Jerry Dallas, the Regional Program Chief, and he was interested in considering it further. As we discussed yesterday, I suggest that you write a letter to Jerry advising him of your interests and requesting a meeting. I would be happy to meet with you and the FAA either in Los Angeles or in the County at a mutually convenient time. The cost for this type of study would vary between \$20,000 and \$30,000 depending upon the depth of analysis you require plus any additional services such as a lease review and rates and charges evaluation. I will discuss this again with Jerry after he has received your letter and try to schedule a meeting regarding the matter.

Mr. Daniel B. Pavao  
September 28, 1979  
Page 2

As you know, we have one more meeting scheduled in our contract and I am prepared to make a trip to El Centro to meet with either the Board of Supervisors or the Planning Commission (or both). Please let me know regarding a schedule.

Best regards,

Michael A. Shutt  
Partner

MAS:er

cc: Mr. Dick Mitchell

TO: Mr. Mitchell

FROM: Jim Kelley

SUBJECT: Proposed development by Don Brown

In reviewing the proposal there are some problems in a couple of areas. The 294 lot subdivision, as I understand it, proposes sewage disposal by way of septic tank. The Health Department indicates that 5 1/2 acres would be necessary for adequate sewage disposal if it were done by septic tanks. At this point they can't say whether they would approve that type of development or not. That particular parcel would require rezoning from Open Space to R-1-T. Also, some of the lots that are shown in the proposal are less than 50' in width; although all of them do have the 3500 square foot minimum lot size as specified in the R-1-T zone. They are also providing approximately 118,000 square feet of commonly owned recreation parcel. That is approximately 1,000 square feet less than the Ordinance specifies.

Concerning the 150 condominiums, I do not have a precise detailed layout plan but it appears as if it could create parking problems and again we may have the same problem with the sewer development. That would also require rezoning from Open Space to R-4 and a subdivision map. The proposed RV spaces would require a Conditional Use Permit, the motel and spa area would require either a rezoning or a Conditional Use Permit.

All of the interior streets appear to be substandard if intended to be County streets. They average 30' in width with the alleys being 20'. Again, it is unknown at this time whether the streets would be publically maintained or private. If they are to be private, that would indicate a Planned Unit Development type concept which we do not presently have an Ordinance for.



The applicants also show a well site. It is unknown at this time whether that well is to serve as potable water for all of the proposal or whether that well is to supply water to the spa area within the motel site. If, in fact, they are proposing to utilize septic tanks, with this amount of development, we could have

Sari

**RECEIVED**

SEP 12 1979

IMPERIAL COUNTY  
PLANNING DEPARTMENT

# FEDERAL ASSISTANCE

1. TYPE OF ACTION

PREAPPLICATION  
 APPLICATION  
 NOTIFICATION OF INTENT (Opt)  
 REPORT OF FEDERAL ACTION

(Mark appropriate box)

2. APPLICANT'S APPLICATION

a. NUMBER

b. DATE Year month day  
19 79 07

STATE APPLICATION IDENTIFIER

a. NUMBER

b. DATE ASSIGNED Year month day  
19 79 07

SCAG Ch Official

4. LEGAL APPLICANT/RECIPIENT

a. Applicant Name : Imperial County, California  
 b. Organization Unit : Imperial County Airport  
 c. Street/P.O. Box : 1101 Airport Road  
 d. City : Imperial  
 e. State : California  
 f. Contact Person (Name & telephone No.) : Daniel B. Pavao

e. County : Imperial  
 g. ZIP Code: 92251  
 (714) 355-2894

5. FEDERAL EMPLOYER IDENTIFICATION NO.

6. PROGRAM (From Federal Catalog)

a. NUMBER 12011021  
 b. TITLE Airport Development Aid Program

7. TITLE AND DESCRIPTION OF APPLICANT'S PROJECT

- o Land Acquisition.
- o Construction of East-West General Aviation Runway and Parallel Taxiway, including necessary marking.
- o Medium Intensity Runway Light System (MIRL) and Visual Approach Slope Indicator (VASI).
- o Perimeter Fencing.

8. TYPE OF APPLICANT/RECIPIENT

A-State  
 B-Interstate  
 C-Substate District  
 D-County  
 E-City  
 F-School District  
 G-Special Purpose District

H-Community Action Agency  
 I-Higher Educational Institution  
 J-Indian Tribe  
 K-Other (Specify):

Enter appropriate letter  D

9. TYPE OF ASSISTANCE

A-Basic Grant  
 B-Supplemental Grant  
 C-Loan

D-Insurance  
 E-Other

Enter appropriate letter(s)  A

12. TYPE OF APPLICATION

A-New  
 B-Renewal

C-Revision  
 D-Continuation

E-Augmentation

Enter appropriate letter  A

10. AREA OF PROJECT IMPACT (Names of cities, counties, states, etc.)  
 Imperial, El Centro, Imperial County, California

11. ESTIMATED NUMBER OF PERSONS BENEFITING  
 85,000

13. PROPOSED FUNDING

a. FEDERAL	\$ 1,484,000 .00
b. APPLICANT	184,200 .00
c. STATE	173,400 .00
d. LOCAL	
e. OTHER	
f. TOTAL	\$ 1,842,000 .00

14. CONGRESSIONAL DISTRICTS OF:

a. APPLICANT 43  
 b. PROJECT 43

15. TYPE OF CHANGE (For 13c or 13e)  
 A-Increase Dollars  
 B-Decrease Dollars  
 C-Increase Duration  
 D-Decrease Duration  
 E-Cancellation

F-Other (Specify):

Enter appropriate letter(s)

16. PROJECT START DATE Year month day  
 19 80 01 01

17. PROJECT DURATION  
 24 Months

19. EXISTING FEDERAL IDENTIFICATION NUMBER  
 N/A

20. FEDERAL AGENCY TO RECEIVE REQUEST (Name, City, State, ZIP code)  
 Administration, Box 45018-Westchester Sta. Los Angeles, CA 90045

22. THE APPLICANT CERTIFIES THAT

23. CERTIFYING REPRESENTATIVE  
 Daniel B. Pavao  
 Airport Manager

a. To the best of my knowledge and belief, data in this preapplication/application are true and correct, the document has been duly authorized by the governing body of the applicant and the applicant will comply with the attached assurances if the assistance is approved.

b. If required by OMB Circular A-95 this application was submitted, pursuant to instructions therein, to appropriate clearinghouses and all responses are attached:

(1) State Clearinghouse  
 (2) Southern California Assn. of Govments

b. SIGNATURE *Michela A. [Signature]*  
 for/ DAN PAVO

DATE SIGNED Year month day  
 19 79 6 22

21. REMARKS ADDED  
 Yes  No

24. AGENCY NAME

25. APPLICATION RECEIVED Year month day  
 19

26. ORGANIZATIONAL UNIT

27. ADMINISTRATIVE OFFICE

28. FEDERAL APPLICATION IDENTIFICATION

29. ADDRESS

30. FEDERAL GRANT IDENTIFICATION

31. ACTION TAKEN

a. AWARDED  
 b. REJECTED  
 c. RETURNED FOR AMENDMENT  
 d. DEFERRED  
 e. WITHDRAWN

32. FUNDING

a. FEDERAL	\$	.00
b. APPLICANT		.00
c. STATE		.00
d. LOCAL		.00
e. OTHER		.00
f. TOTAL	\$	.00

33. ACTION DATE Year month day  
 19

34. STARTING DATE Year month day  
 19

35. CONTACT FOR ADDITIONAL INFORMATION (Name and telephone number)

36. ENDING DATE Year month day  
 19

38. FEDERAL AGENCY A-95 ACTION

a. In taking above action, any comments received from clearinghouses were considered. If agency response is due under provisions of Part 1, OMB Circular A-95, it has been or is being made.

b. FEDERAL AGENCY A-95 OFFICIAL (Name and telephone no.)

37. REMARKS ADDED  
 Yes  No

OFFICE OF THE GOVERNOR  
OFFICE OF PLANNING AND RESEARCH  
STATE CLEARINGHOUSE  
1400 - 10TH STREET  
SACRAMENTO, CA 95814

IMPERIAL COUNTY  
1101 AIRPORT ROAD  
IMPERIAL CA 92251  
ATTENTION: DANIEL B. PAVAO

ACKNOWLEDGEMENT

06/30/79  
REPORT IMD45A

PROJECT NOTIFICATION AND REVIEW SYSTEM  
OFFICE OF THE GOVERNOR  
(916) 445-0613

PROJECT: CROSSWIND RUNWAY PROJECT

STATE CLEARINGHOUSE NUMBER (SCH) 79070214

PLEASE USE THE STATE CLEARINGHOUSE NUMBER ON FUTURE CORRESPONDENCE  
WITH THIS OFFICE AND WITH AGENCIES APPROVING OR REVIEWING YOUR PROJECT

DATE RECEIVED: 79/06/27

DATE REVIEW PERIOD ENDS: 79/07/27

THIS CARD DOES NOT VERIFY COMPLIANCE WITH PREAPPLICATION AND/OR  
ENVIRONMENTAL DOCUMENT REVIEW REQUIREMENTS. A LETTER CONTAINING THE  
STATE'S COMMENTS OR A LETTER CONFIRMING NO STATE COMMENTS WILL BE  
FORWARDED TO YOU AFTER THE REVIEW IS COMPLETE

PLEASE CONTACT THE CLEARINGHOUSE IMMEDIATELY IF YOU DO NOT RECEIVE  
THE LETTER BY THE END OF THE REVIEW PERIOD.



# Memorandum

To : Ann Barkley, Chief  
Division of Transportation Planning  
Department A-95 Coordinator  
Attention Art Lichtman

Date: July 16, 1979

File : State Clearinghouse

Imperial County Airport  
Imperial County

From : **DEPARTMENT OF TRANSPORTATION**  
Division of Aeronautics

Subject: Project Review - SCH #79070214P - Crosswind Runway Project -  
Imperial County Airport - Draft Environmental Impact Report

The Department of Transportation, Division of Aeronautics, has reviewed the subject environmental documentation and has no specific comments to submit.

The project, if implemented, will have some adverse environmental impacts which are unavoidable. However, it is believed that safety of airport operations will be improved. This fact, coupled with the measures proposed to mitigate significant impacts indicates that benefits to the public outweigh any disbenefits to the environment.

The construction and operation of the proposed new crosswind runway will require an amended airport permit from the Department. This places us in the role of Responsible Agency with discretionary approval power over the project. Consequently, before the amended airport permit can be issued, the Lead Agency must comply with the provisions of Section 15085(h)(3), State EIR Guidelines. The Department would appreciate a copy of the Notice of Determination, bearing the date it is duly filed with the Secretary for Resources.

Thank you for the opportunity to comment.

G. A. MILLER  
Deputy Chief

Attachment



EDMUND G. BROWN JR.  
GOVERNOR

# State of California

GOVERNOR'S OFFICE  
OFFICE OF PLANNING AND RESEARCH  
1400 TENTH STREET  
SACRAMENTO 95814  
(916) 445-0613

July 27, 1979

Daniel B. Pavao  
Imperial County  
1101 Airport road  
Imperial, CA 92251

Subject; SCH# 79070214 Crosswind Runway Project

Dear Mr. Pavao:

State review of your federal grant application and related environmental document is complete. The comments by state agencies are attached.

Under the provisions of Circular A-95 of the federal Office of Management and Budget and of Section 0911 of the State Administrative Manual, you are required to respond to comments on your grant application. The California Environmental Quality Act requires you to respond to comments on the environmental document.

Your compliance with Circular A-95 is complete providing you respond to the comments on your grant application.

Sincerely,

  
Stephen V. Williamson  
State Clearinghouse

SVW/ag





600 South Commonwealth Avenue • Suite 1000 • Los Angeles • California • 90005 • 213/385-1000

DATE: July 19, 1979

TO: Mr. Daniel B. Pavao  
Imperial County Airport  
1101 Airport Road  
Imperial, California 92251

FROM: Metropolitan Clearinghouse

SUBJECT: Land Acquisition, Construction of Runway, Lighting  
SCAG File Number IM-10578-AP

As required by OMB Circular A-95, we have disseminated information on your proposed grant application to cities, counties and some special agencies in the region which may be interested in the project. Also, the project has been reviewed by the SCAG staff to determine the relationship of the project to adopted regional policies, plans and programs. Comments generated through the A-95 review process are listed below and should be attached to the grant request when it is submitted to the funding agency.

The SCAG staff review found that:

1. The Imperial County Airport project is of regional significance and is consistent with the SCAG adopted Regional Transportation Plan.
2. The SCAG Aviation and Technical Advisory Committee recommended favorable comments at their meeting of July 6, 1979.
3. No comments have been received in response to the inclusion of this project on the Bi-Weekly Clearinghouse Listing.

A handwritten signature in cursive script, appearing to read 'Leslie Cook', is written over a horizontal line.

Clearinghouse Official

LK:LA:cd

CITY COUNCIL

CITY OF IMPERIAL

OFFICERS

Patricia Burk  
Michael Kelley, MAYOR  
Tom Ludlow  
Leamon Murphy  
Dean Shores  
Betty Willlett, Clerk

Incorporated 1904  
Home of  
Imperial County Fair  
Imperial County Airport  
Headquarters of Imperial Irrigation District  
Telephone 355-4372  
420 South Imperial Avenue  
IMPERIAL, CALIFORNIA 92251

Betty Willlett, City Clerk  
Lawrence Eager, Treasurer  
J. Penn Carter, Attorney  
Paul M. Wheeler, Chief of  
Police and Fire Chief  
Fred K. Boyd,  
Superintendent of Public Works

RECEIVED

AUG 24 1979

IMPERIAL COUNTY  
PLANNING DEPARTMENT

AUGUST 20, 1979

IMPERIAL COUNTY BOARD OF SUPERVISORS  
COURTHOUSE  
EL CENTRO, CALIFORNIA 92243

GENTLEMEN:

IN REFERENCE TO THE DRAFT ENVIRONMENTAL IMPACT REPORT ON THE CROSSWIND RUNWAY PROJECT AT IMPERIAL COUNTY AIRPORT, AS PREPARED BY HODGES & SHUTT, AVIATION CONSULTANTS, THE FOLLOWING COMMENTS ARE FURNISHED:

ON AUGUST 15, 1979, THE CITY COUNCIL OF THE CITY OF IMPERIAL UNANIMOUSLY EXPRESSED ITS CONCERN ABOUT THE OMISSION OF REFERENCE TO OR SPECIFIC PROVISION FOR DRAINAGE OF SURFACE WATERS WHICH MAY ACCUMULATE IN THE PROJECT AREA.

TYPICALLY, THESE WATERS ARE OF THE THUNDERSTORM VARIETY CAUSING HEAVY, CONCENTRATED RUNOFF DURING SHORT PERIODS OF TIME.

BECAUSE OF THE FLAT TERRAIN, THIS COMMUNITY IS ALREADY ADVERSELY IMPACTED BY THESE RAPID RUNOFFS (FROM THE SOUTH) ALONG STATE HIGHWAY 86.

THEREFORE, WE REQUEST THAT THE FINAL E.I.R. ADDRESS THE SPECIFIC ISSUE EXPRESSED ABOVE; THAT MITIGATING MEASURES BE PROPOSED THAT WILL DRAIN THE PROJECT AWAY FROM HIGHWAY 86, SUCH AS:

- (1) GRADED SURFACE DRAINAGE CONNECTING TO DRAINAGE OUTLETS AT DAHLIA PIPELINE DRAIN AND NORTH CENTRAL DRAIN NO. 2,
- (2) PONDING OF SURFACE DRAINAGE IN AREAS SO DESIGNED,
- (3) A COMBINATION OF THE ABOVE, OR
- (4) OTHER.

SINCERELY YOURS,

*Michael W. Kelley*

MICHAEL W. KELLEY  
MAYOR OF THE CITY OF IMPERIAL

CC: HODGES & SHUTT, CONSULTANTS  
AIRPORT MANAGER, IMPERIAL COUNTY AIRPORT  
IMPERIAL COUNTY PLANNING DEPARTMENT ✓

NOTICE OF PUBLIC HEARING  
TO DISCUSS  
THE PROPOSED EAST/WEST RUNWAY

Notice is hereby given that a public hearing will be held Wednesday, August 8, 1979, by the Airport Land Use Commission, in the Conference Room, Imperial County Airport, Imperial, California at 7:35 p.m., to hear a discussion on the proposed East/West Runway for the Imperial Airport and for a discussion of the Draft Environmental Impact Report prepared for the project.

A full Draft Environmental Impact Report has been prepared for the project and is available.

All interested persons are invited to attend and be heard.



---

RICHARD D. MITCHELL, Secretary  
Airport Land Use Commission

1 TRANSCRIPT OF HEARING

2 AIRPORT LAND USE COMMISSION

3 August 8, 1979

4  
5 COMMISSIONERS:

6 BOB BUCHER, Chairman

7 KEN BEMIS

8 E. L. LIGHTER

9 DAVID PRICE

10 DEAN SHORES

11 DAN PAVAO

12 DON PRESTON (Absent)

13  
14 MR. BUCHER: A public hearing to discuss the proposed East/West  
15 runway of the Imperial airport and so, at this time, I would like to  
16 declare the hearing open and I would like to turn the meeting over  
17 now to Mike Shutt who is connected with the consultant, EIR - and I  
18 will turn the meeting over to him at this time.

19 MIKE SHUTT (Consultant): Thank you Mr. Chairman. What I would  
20 like to do before we get into the official testimony for the EIR is  
21 to take a few minutes and briefly go through the plan as we are pro-  
22 posing it and as the Environmental Report depicts it. What we are  
23 discussing in the Environmental Document is the new runway for the  
24 airport located in an East/West alignment 4500 foot in length, 75  
25 foot wide with a parallel taxiway and other associated ancillary  
26 facilities such as lights, visual approach aids and other landing  
27 aids associated with the runway.

28

1 The project would entail the acquisition of some 75 acres of  
2 land that is now not being used for airport purposes, would involve  
3 the construction of security fencing around the runway as we are  
4 proposing it. Involves the, a fairly unique aspect of the project is  
5 that LaBrucherie Road will be closed by the construction of this runway.  
6 As a mitigation measure, it is proposed that Brewer Road be extended  
7 to the West to connect with Austin Road in order to continue to pro-  
8 vide the North/South flow of traffic that is currently using LaBrucherie  
9 Road and that would be a part of the project as a mitigation measure  
10 for the disruption to the traffic on LaBrucherie.

11  
12 Another aspect of the project, a unique aspect, is that with the  
13 new Crash Fire Rescue Station and County Fire Services located next  
14 to the control tower, it has been necessary to plan in radio control  
15 gates in order to allow the fire service to still access the area  
16 South of the airport by using LaBrucherie Road so there would be no  
17 disruption to fire protection to the County by the fact that this pro-  
18 ject would cross and essentially sever LaBrucherie Road.

19  
20 We are looking at a project now that is estimated to cost 1.8  
21 million dollars in 1979 costs. The project would be extensively funded  
22 by the Federal Aviation Administration through user supported tax funds  
23 from the Airport Aid to Development Program.

24  
25 This is a project that had a demonstrated need back in 1974  
26 when the County's Airport Plan was originally developed. There has  
27 been some differences in the definition of the project over the past  
28 few years. The runway location that we are proposing and that has

1 been prepared in the report is now located about 3000 feet further  
2 South than that location which was initially shown. The initial  
3 alignment fronted on the town of Imperial and it was located in that  
4 position primarily to accomodate air space constraints with the Naval  
5 Air Facility. Since then, the acquisition of the control tower at  
6 this airport and a fairly extensive communication network between this  
7 airport and the Naval Facility has virtually eliminated the airspace  
8 conflicts between the two facilities and it is understood that with  
9 limited use of the crosswind runway by lighter aircraft, general  
10 aviation type aircraft, there would continue to be no adverse conflicts  
11 between the two facilities and their operating procedures. Another  
12 reason that there has been a change in the definition of the project  
13 is that since 1974 there have been some very major changes in the  
14 airline industry. As you all know, Airwest is no longer serving  
15 the community. The original Master Plan contemplated their continued  
16 service and ultimate service into this community with DC-9 jet aircraft.  
17 Now, in this community and in several other communities in the country,  
18 the regional service that was originally provided by Airwest is now  
19 being provided by commuter airlines using much smaller equipment than  
20 what the regional carriers were initially using. Therefore, we looked  
21 at a runway that could accomodate the needs of these operators in the  
22 times of the strong crosswinds - so what was originally a runway about  
23 3800 feet long on the original Master Plan now has been extended to  
24 4500 feet in length due primarily to the feeling that it is necessary  
25 to accomodate these metroliners and various other aircraft that will  
26 be providing service to the community for the foreseeable future.

27  
28 As far as environmental impacts on the project, I would like to go



1 through the list of the items that we looked at. One of the impacts  
2 of the project is that about 60 acres of agricultural land will be taken  
3 out of production in the area and this is in conflict somewhat with the  
4 County's General Plan to preserve agricultural use. With respect to  
5 circulation, LaBrucherie Road will be severed by the project. Mitigation  
6 measures would include the construction of the link from Brewer over  
7 to Austin. However, for those people who do use LaBrucherie Road  
8 and the traffic counts indicated about 1,000 cars a day, 500 each  
9 way, there would be some inconvenience and some additional driving  
10 time to make the circuit from the town of Imperial down to El Centro.  
11 In fact, that was the intended point that people were going to.

12  
13 As far as the social impact of the project, this construction  
14 project would require the relocation of one dwelling unit off of  
15 LaBrucherie Road. The project takes into account the fact that the  
16 people who are renting this unit will have to be taken care of in  
17 accordance with the Uniform Relocation Act of the Federal Government  
18 and there is a relocation plan that has been made a part of the En-  
19 vironmental Document and the mitigation for the social impact here  
20 will be the fact that the Federal law does require compliance with  
21 that particular law.

22  
23 With respect to flora and fauna, wildlife and plants, we have  
24 found virtually nothing that would be significantly impacted in the  
25 area. The plant life and animal life is not unique and there's much  
26 replacement area within the County proper that would be converted  
27 over to Airport use.

28



1 With respect to noise, we found virtually no significant change  
2 in the noise impact of the airport and, if anything, the project pro-  
3 bably will reduce the overall noise impact of the airport upon residen-  
4 tial development in the area. I say probably because we don't really  
5 know for sure how much use the runway would get but it appears that  
6 any use that it gets would take activity off of the North/South runway  
7 where aircraft who are departing to the North do emit noise over the  
8 town of Imperial and would put those airplanes on a track then that  
9 would take them to the West where they're virtually impacting no  
10 residential units in that area. So, if anything, we feel that the  
11 project does nothing to increase the ambient noise problem and, if  
12 anything, it might even mitigate the existing airport environment.

13  
14 Air quality. Again, we found no significant impacts with respect  
15 to air quality even though the area is in an attainment area for  
16 particulates. Aircraft emissions are so miniscule compared to other  
17 sources of emissions that they make up a very very insignificant per-  
18 centage of total emissions within the County and with or without the  
19 project there would be virtually no change at all in the air quality.

20  
21 Public safety. We believe that the project itself is intended  
22 to enhance public safety. The high crosswinds that occur at this  
23 airport come out of the West and they come out of the West in the  
24 Spring months at a very very high velocity and there is a record  
25 and history of accidents on the airport where aircraft have attempted  
26 to land in that crosswind and have experienced a recordable accident  
27 because of it. So, the project itself is intended to enhance public  
28 safety.

1 Now, with respect to the other item I mentioned before about  
2 the fire service. We believe that mitigation can be built into the  
3 project to allow the fire department to continue to serve the County  
4 with no delays in their response time to anywhere in the County.

5  
6 As far as energy goes, this is the type of project that, outside  
7 of the energy cost of constructing it, there are no long term impacts  
8 we can conceive as far as the energy consumed because or of the project  
9 or because of not implementing the project. Again, we feel that is  
10 kind of a wash.

11  
12 So, in general, that is the project definition, how its per-  
13 ceived in the Environmental Document and a summary of the impacts  
14 as our firm, as the consultants to the County, have presented them  
15 and with this I would like to turn the meeting back over to the Chairman  
16 who will then either entertain unofficial comments from the Land Use  
17 Commission or official documented comments from the public. Before I  
18 do that, I will explain what is being done. This will become a part  
19 of the official Federal document that will be circulated through the  
20 Federal Aviation Administration in order to secure their funds for this  
21 project should the County decide to go ahead and implement it. So,  
22 the hearing is an intergral part of that process and any comments that  
23 are brought up tonight will be recorded. If there are quesions to be  
24 answered, they most likely will not be answered tonight but they will  
25 be answered at a later time and will be made a part of the Environmental  
26 Document. So that is essentially the purpose of what we are here  
27 tonight to gather anyway. Thank you Mr. Chairman.

1 RICHARD MITCHELL (Planning Director): Just to clarify one thing;  
2 the question is not whether the project should be approved or not but  
3 is the question of what environmental impacts the project will have.  
4 Is that correct?

5 MIKE SHUTT: That's correct. Positive and negative; those who  
6 oppose or support.

7 BOB BUCHER: Okay. Thank you Mike. Then we will go on now with  
8 the informal comments from the Board. Have we any comments at this  
9 time? I do have one I would like, I will start off with. Mike, now  
10 on page 36 it said something about the annual rain, 3.5 feet of water  
11 will runoff the paved area in a year. Now, I have served on the City  
12 Council for six, seven, maybe eight, seven years I guess and we've  
13 had some rains that we just really had water backed up in our City.  
14 Now, I'm wondering is it the plan to incorporate the drainage of that  
15 area into the existing drains in that area or how do they propose  
16 doing this? Have you any idea at this time?

17 MIKE SHUTT: We haven't done a thorough engineering evaluation  
18 of how the drainage would be handled. Its tentatively proposed that  
19 it would be handled in the existing . . .

20 BOB BUCHER: In the existing . . .

21 MIKE SHUTT: . . . storm water drain

22 BOB BUCHER: Okay

23 MIKE SHUTT: . . . and the additional runoff from the paved area  
24 is what would be incrementally added to the drainage system as opposed  
25 to what would be flowing into the drain system right now.

26 BOB BUCHER: Well, the reason I ask is that I am sure hoping you  
27 won't rely on this drain ditch out along Highway 86 because it sure  
28 got the City in trouble and I suspect it will still continue doing so

1 until other steps are taken but this was the only qualms I had against  
2 going through the EIR that bothered me and if they will take care of that  
3 why I'm, I can find noxhing else wrong with it.

4 MIKE SHUTT: Well, may I suggest that maybe as a measure of  
5 mitigation for that during the actual designs if it becomes apparent  
6 that there is a problem, some other alternatives could be investigated  
7 such as ponding on the airport; some storage on the airport if the  
8 runoff is excessive. I might suggest you add that to a comment if  
9 you want that considered.

10 DEAN SHORES: You might note that there is another drain

11 BOB BUCHER: We're going to be crossing two drain ditches

12 DEAN SHORES: That's right but there is a . . .

13 BOB BUCHER: LaBrucherie and then . . .

14 DEAN SHORES: . . . more adequate drain

15 BOB BUCHER: There would be but what I'm saying is that in the past,  
16 in the last couple of years, we've had quite a problem with this ditch  
17 filling up and we did have a lot of drainage off the airport and I'm  
18 hoping that they will do somthing about that and not rely on this  
19 drain ditch out along Highway 86. It can sure be a very big problem.  
20 Matter of fact we're in a law suit right now, still.

21 DEAN SHORES: Two law suits

22 BOB BUCHER: Two law suits still over this very problem. Dick,  
23 did you want to say something?

24 RICHARD MITCHELL: The existing runway has no surface runoff  
25 plan, so any plan for the East/West runway should have some place  
26 for the water to go. Goes out off between the taxiway and runway  
27 and there's a few sumps there that eventually take it away if it  
28 can, it its running and so on, so you don't need that. Another



1 question I had, what weight aircraft are you planning your East/West  
2 runway for?

3 MIKE SHUTT: The EIR calls out a 35,000 pound gross weight air-  
4 craft as the ultimate design aircraft. Initially, the aircraft used  
5 in the airport weigh about 15,000 pounds which is the weight of the  
6 metroliner but on the drawing boards right now are aircraft that would  
7 serve this type of market with a 30 passenger airplane and their design  
8 limits are up to 35,000 pounds and we would propose designing this  
9 runway to accomodate that future type of aircraft.

10 RICHARD MITCHELL: But, it is designed to go up to heavier weight  
11 by increasing the thickness and things like this?

12 MIKE SHUTT: Yes, there would be nothing to preclude it from going  
13 to a heavier aircraft. It would just be the cost of the asphalt.

14 BOB BUCHER: Anybody else?

15 DAVID PRICE: From what I read in your, now we're talking about  
16 possibly using this runway at 12 knotts or greater

17 MIKE SHUTT: The purpose of the runway is to provide relief when  
18 that occurs and whether or not the runway would be used at other  
19 times, say during the calm wind condition, would probably be more of  
20 a matter of airport policy and user convenience; whether someone wants  
21 to taxi down and use that runway or land on it and taxi back. That's  
22 why we can't really fully estimate how much of the time it will be  
23 used. We know during the high crosswinds it would be but also during  
24 some calm winds there might be some demand for it.

25 BOB BUCHER: Anybody else? Okay, I would like to open the meeting  
26 then to the public to any comments that they may have at this time.  
27 Pros or otherwise.

28 HENRY WORTHINGTON (From Audience): Mr. Chairman

1 BOB BUCHER: Yes

2 JACQUELINE KING (Secretary): Excuse me

3 BOB BUCHER: Yes, Mr. Worthington. Yes, if you would come closer  
4 to the mike.

5 JACQUELINE KING: I'm going to need a name

6 JIM KELLEY (Zoning Administrator): Henry Worthington

7 HENRY WORTHINGTON: From a selfish standpoint, naturally, I would  
8 be - object to it but, of course, in the interest of the whole community  
9 well, of course I would listen but its very obvious that I will be bearing  
10 the brunt of this whole thing, going through my ranch like it is and  
11 it, it really is quite a blow but I realize that progress will just  
12 have to have precedent and so my question comes to me after I heard  
13 the presentation here. Is this thing really necessary? Is there any  
14 other alternate whereby it could be accomplished? Just a question I  
15 wondered.

16 BOB BUCHER: Well, Mr. Worthington, about all I can say, according  
17 to the EIR, there are two other airports but Holtville is at a point  
18 where the cost would be greater to go in and to move this airport there  
19 than it would be to do the East and West runway and, of course, then we  
20 have the Navy base out here which is, is really not the answer either  
21 and it seems like the most logical way, according to the EIR, to go  
22 at this time. Mike, have you anything to add to that?

23 MIKE SHUTT: I would just refer Mr. Worthington to the Alternative  
24 Section of the Report. There were several alternatives evaluated and  
25 they're fairly well documented in that section and Mr. Chairman has  
26 elaborated on them. The Naval Air Station, which has expressed re-  
27 luctance in taking on the smaller aircraft, and Holtville, which is  
28 a fairly expensive proposition to totally develop, so it leaves us with



1 the County airport and what can be done to the County airport. Three  
2 alternatives of the County airport were reviewed as to where the runway  
3 could be located there in an East/West direction.

4 HENRY WORTHINGTON: Well, that's just a few words that I would  
5 like to say

6 MIKE SHUTT: And, may I interject; one other thing in the Report  
7 there's also some mitigation as far as the impact upon your ranching  
8 operation. It is recommended that agricultural crossings be prepared  
9 for your farm equipment. That where there is severence between the  
10 fields in order that implements can make the circuit from one field to  
11 another without having to go up on Austin Road and around and that  
12 would be part of the project.

13 HENRY WORTHINGTON: Okay

14 BOB BUCHER: Yes

15 JOHN SCHMIDT (From Audience): I've just got a quick . . . I rep-  
16 resent Braden Machinery Company

17 RICHARD MITCHELL: May we have your name?

18 JACQUELINE KING: Excuse me, I'm going to need a name

19 JOHN SCHMIDT: John Schmidt. My question is that on this map  
20 it shows the future clear zone of 500 feet. Is there any possible way  
21 to move that on the other side of this taxiway?

22 BOB BUCHER: Well, that's, isn't that taking the right of the  
23 extreme North end of Mr. Braden's property there?

24 JOHN SCHMIDT: Yes, it shows 500 feet here

25 BOB BUCHER: About 500

26 JOHN SCHMIDT: But where they drew the line, this is on the out-  
27 side of it. In other words, if they wanted to purchase some of this  
28 property, would be purchasing this? Is that right?

1 MIKE SHUTT: Be purchasing this 500 foot of the total width of  
2 this part and I believe this is 250 feet from the centerline to the  
3 property

4 JOHN SCHMIDT: The 500 feet is in this

5 MIKE SHUTT: This is a better map. This 500 feet is here

6 JOHN SCHMIDT: Oh, its inside this line here

7 MIKE SHUTT: This line here is what we mentioned

8 JOHN SCHMITT: Right, right

9 MIKE SHUTT: The 250 feet that would be the recommended property  
10 line to the South

11 JOHN SCHMIDT: I see

12 RICHARD MITCHELL: Please, please speak into the microphone

13 JOHN SCHMIDT: Another question I wanted to ask

14 MIKE SHUTT: Why don't you ask this question again and then we  
15 will get that on record

16 JOHN SCHMIDT: This question here?

17 MIKE SHUTT: Yes, ask that question again

18 JOHN SCHMIDT: About this future clear zone here at 500 feet  
19 and you say its inside of this boundary line, the way it shows on  
20 your map, right?

21 MIKE SHUTT: That's correct

22 JOHN SCHMIDT: It isn't outside?

23 MIKE SHUTT: The property line would be located 250 feet from the  
24 runway centerline and the clear zone would be on the airport property

25 JOHN SCHMIDT: So, it would be inside of the area that you would  
26 require

27 MIKE SHUTT: That's correct

28 JOHN SCHMIDT: The other question I have is that supposing we made

1 some arrangements to . . .

2 MIKE SHUTT: I need you to talk into one of the mikes for the  
3 tape recorder

4 JOHN SCHMIDT: Oh, supposing that we made some arrangements to  
5 build hangers on the balance of our property. Would there be any  
6 chance in having taxiways into the airstrip? Was that going to be  
7 fenced?

8 DAN PAVAO: I'd say that that would be a policy decision made by  
9 the Board of Supervisors rather than the consultant and in the EIR.  
10 We've never obviously had that situation up until this time. I  
11 wouldn't foreclose the possibility and yet I can't say that, I won't  
12 say that its possible at this time. We would have to look at it.  
13 there would be some, some particular arrangements that we would have  
14 to make concerning the fencing itself and either leasehold or rentals  
15 of one kind or another would be to the County one way or the other  
16 or the property owner

17 JOHN SCHMIDT: Okay, that's all the questions I have

18 BOB BUCHER: Mr. Shank

19 DAVID SHANK (From Audience): My name is David Shank. I own the  
20 property just . . .

21 JIM KELLEY: Excuse me, would you give your address please?

22 DAVID SHANK: Okay, its 2589 Nance Road, corner of Nance and Brewer.  
23 What I was curious about, on the map that I got from the consultants,  
24 that they were to widen out the existing runway. Okay, one thing I've  
25 been wondering about. Is that going to put Mr. Worthington's house  
26 within the sound pattern also of the runway? If they widen out the  
27 existing runway, if they will close LaBrucherie Road at Mr. Worthington's  
28 house on the corner of Brewer and LaBrucherie well, where abouts are

1 they going to close LaBrucherie Road?

2 MIKE SHUTT: There are a couple of questions there. I'll answer  
3 the second one first. LaBrucherie Road would be closed approximately  
4 at the airport boundary where the runway would cross the road. That would  
5 be South of the control tower. As far as the widening of the existing  
6 runway, that is, that is merely shown on the County's airport layout  
7 plan which has been on the County's airport layout plan for the past  
8 seven years and that project is not really dependent one way or another  
9 upon what's being discussed tonight.

10 DAVID SHANK: Okay

11 MIKE SHUTT: And I don't have an answer as to what that does to  
12 Mr. Worthington's property.

13 DAVID SHANK: Another question that's been in my mind when the  
14 people come around and ask me about it. I can't remember the gentleman's  
15 name but I think he said it's estimated at what, 1.8 million dollars.

16 MIKE SHUTT: That's correct

17 DAVID SHANK: Does anybody have any, you said yourself that you  
18 actually don't have any idea how much use there is going to be on the  
19 runway considering wind and light aircraft and, for instance, so  
20 all kinds of variable factors. Right?

21 MIKE SHUTT: We are estimating that there would be a minimum of  
22 20% use on the runway because the winds favor that direction at least  
23 20% of the time and whether it's more than that or not we don't know

24 DAVID SHANK: Now, what I'm beginning to wonder is, is, just a  
25 second (pause). What I'm beginning to wonder is that 1.8 million  
26 dollars how many people are going to get the use out of this? Is the  
27 County of Imperial going to have to pay this money back or is it just  
28 given to them by the Federal government?



1 MIKE SHUTT: The airport aid, the Airport Development Aid Program  
2 that funds this right now sits with approximately 3 billion dollars  
3 in a trust fund that is gathered primarily from ticket taxes. Every-  
4 time you board an airplane out of here you pay, I believe it is a 6%  
5 ticket tax that goes into this fund. It is that money, along with the  
6 money that comes in from the sale of aircraft fuels and tires and  
7 avionics, that goes into this trust fund and its the trust fund monies  
8 that will be used to fund this project. The Federal government would  
9 put up 80% of the project approximately; the County would be responsible  
10 for providing the additional 20% to make it whole. And, as far as those  
11 that would benefit from the project, its our estimation that when these  
12 high crosswinds are occuring, anybody who will fly into this airport  
13 on the commuter airlines or on private, person or business trips will  
14 be using that runway and would benefit from its location.

15 DAVID SHANK: Well, I - I just like everybody else you can't put  
16 a dollar figure on somebody's life but I'm just beginning to wonder  
17 is somebody footing this bill and I'm beginning to wonder if little  
18 projects like this are actually worth that kind of money. Now, like  
19 I say, you can't put a dollar figure on anybody's life but I'm beginning  
20 to wonder if stuff like this is really worth it. I mean, you know its  
21 just little things like this that just keep running taxes and taxes  
22 and taxes on people and you know it - it really doesn't figure to me  
23 if its really worth it to put that kind of money out. Its just like I  
24 told everybody before when you first started this thing about five or  
25 six years ago that if they was going to do it, they should've just done  
26 it and got it over with instead of running through all the little  
27 goodies and everybody fighting about it and Mr. Shores went through it  
28 and everybody along all the little deals out here went through it

1 that if it was going to been done, it should've just been done when  
2 everybody started talking about it and got it over with. Myself,  
3 personally, I cannot see to where a project like this is actually  
4 worth the money that people are talking about putting out for it.

5 DEAN SHORES: May I say something? The, you talk about putting a  
6 dollar value on somebody's life

7 DAVID SHANK: Right

8 DEAN SHORES: I think that's very important. But, if you've  
9 worked around the airport, I've worked on this airport for better than  
10 20 years and yes, its worth it. Its worth having the East/West runway.  
11 If it saves one life, its worth it and we have had numerous  
12 accidents - I don't know how many total, but we've had numerous  
13 accidents caused because we did not have an East/West runway. Right  
14 now there are many pilots coming in using the taxiway at the North end  
15 of the airport and when they call in to use that, if they're not aware  
16 of it, the tower does not make them aware of it and if they do use it  
17 they notify them that they are using it at their own risk. Now, I  
18 don't know where that makes the insurance companies stand; I have  
19 no idea, but this runway here, especially if you're talking about  
20 putting in airliners, the small metroliner and so forth, this has  
21 got to be a safer method of putting the people on the ground and  
22 lifting them off than what exists today at this airport. When you  
23 talk about waste of taxpayer's money, when you look at the projects  
24 where real waste exists, as an example now if you've been down to the  
25 employment office you see some sculpture being down down there - has  
26 been done - you compare that to something that could save someone's  
27 life. Yes, its worth it. Its not a waste, not in my opinion.

28 DAVID SHANK: Okay



1           DAVID PRICE: Can I add something to that? Representing the  
2 general aviation, most of this money is not out of the tax dollars.  
3 Its produced by the general aviation and what they pay for gas, tires  
4 and things such as this plus the tax on tickets and evidently there's  
5 3 billion dollars around that is not basically being utilized for  
6 general aviation

7           DAVID SHANK: Basically the same thing as gasoline taxes

8           DAVID PRICE: But it is not coming out of the general tax. These  
9 people are the ones producing the funds for it

10          DAVID SHANK: Right. Okay, now . . .

11          DEAN SHORES: I think he was really speaking about that 20%  
12 that the County's going to have to come up with

13          DAVID SHANK: Well, its not, I mean its just basically the general  
14 overall 1.8 million dollars that you're talking about. Now I think  
15 if I'm not mistaken Howard Worthington said something about two or three  
16 years ago that if they just quit talking about it and do it, it would've  
17 cost them a whole lot less

18          DAVID PRICE: Oh, I agree

19          DEAN SHORES: If they'd have done it 20 years ago when they first  
20 started talking about it, it would have cost them a whole bunch less  
21 too

22          DAVID SHANK: That's another deal. Okay, now I don't know if this  
23 has anything to do with what this meeting's about, if its not, just  
24 let me know about it. I talked to you about it the last time. For  
25 instance, where the EIR says there would be actually no sound variation,  
26 if anything it will lighten up on Imperial, right. Now, Mr. Shores,  
27 you know where my property is, I talked to you about it last time.  
28 How will that have anything to do with my property being 3000 feet to

more punch better

3 DAVID SHANK: Okay, well that's all I wanted to know

4 DEAN SHONES: What can I say, the other one was going to come  
5 right through your living room

6 BOB BUCHER: Another thing. That if they fly the proper pattern,  
7 it looks to me like you're going to have a lot better clearance going  
8 East and West than you will going to the existing runway.

9 DAVID SHANK: I, I'm all for this. I mean actually, but what  
10 I was saying to start with is they could justify it and, like you said,  
11 you know one life is worth it and okay fine, I'll buy that. Then,  
12 it puts me in a position to where I'm wondering about my property as  
13 to what I can do with my property and I've already talked to Jim about  
14 it as subdivision of my property and we got into a deal with Mr. Webster's  
15 property. Now, what kind of light does that put me into if you can  
16 elaborate on it or do whatever

17 JIM KELLEY: Well, number one, you've never applied for, for  
18 anything

19 DAVID SHANK: Right

20 JIM KELLEY: Number two, at the time you were talking about it,  
21 I indicated to you that it would probably be better if you waited until  
22 a formal decision was made as to where the runway was going to go  
23 because at that time we had too many unknowns. We didn't know what  
24 we were dealing with and I'd still have to say the same thing. Until  
25 that formal decision is made as to where that runway is going to go,  
26 I cannot give you a decision, not even a decision. I can't make that  
27 decision. The Planning Director can because of your zoning but I can't  
28 give you an idea as to what your chances of being able to subdivide

1 that property are

2 DAVID SHANK: You can or you can't

3 JIM KELLEY: I cannot until the formal decision is made as to which  
4 alignment that runway will take

5 DAVID SHANK: Can anybody give me a time? A date?

6 JIM KELLEY: I can't

7 DAVID SHANK: I mean in front of the Board of Supervisors

8 RICHARD MITCHELL: It will be by the Board of Supervisors when  
9 the EIR is final and the project is ready for submittal

10 DAVID SHANK: Do you have any idea, can anybody give me a date?  
11 Any kind of date whatsoever?

12 DEAN SHORES: Months

13 RICHARD MITCHELL: I'd say three months. Two to three months, I  
14 would say

15 DAVID SHANK: Two to three months. Okay

16 DEAN SHORES: Add 20% for inflation

17 DAVID SHANK: Yea, right. Thank you I have nothing else to say

18 BOB BUCHER: Is there anyone else in the audience at this time?

19 HOWARD WORTHINGTON: Howard Worthington. I kind of wear a couple  
20 of hats on this deal

21 JIM KELLEY: Excuse me Howard. Give your address please

22 HOWARD WORTHINGTON: Imperial, California. As far as looking  
23 from a pilot's standpoint why, an East/West runway is, looks very  
24 attractive and I think there's several pilots in the room. Second, the  
25 remark that the preceeding gentlemen made if it had been done maybe  
26 two or three years ago I'm afraid maybe it'd gone through some other  
27 different property because at that time there wasn't even a thought  
28 of a different location and I think that at the present location is

1 the most practical location that has been thought of yet and I think  
2 you'll find the least objectionable. True, it cuts up our ranch  
3 operation and we made a statement several years ago, at least I did,  
4 that we wouldn't argue or fight them if the airport project, if we  
5 were sold that it was of necessity. I know the necessity has been  
6 questioned. As I say, I can use it anytime the wind blows from the West  
7 but I know its a costly deal and will inconvenience our ranching  
8 operation but those are things that you have to live with - with pro-  
9 gress

10 BOB BUCHER: Anybody else? Is there anything else from the  
11 members of the Commission?

12 RICHARD MITCHELL: One thing, a request. Mr. Shutt has previously  
13 indicated that we will have to revise the noise pattern around the  
14 existing airport. This will modify the area of influence of the  
15 existing runway and it will be changed with the new runway so as soon  
16 as this EIR gets adopted and the Board makes up its mind, we will -  
17 we will have to come back in and revise our Noise Element and the  
18 pattern around the airport so this causes other changes to take place  
19 and will open up areas for development and might close up some others

20 BOB BUCHER: Anybody else?

21 JIM KELLEY: Mr. Chairman, I have one question for Mr. Shutt.  
22 Mr. Shutt, in looking at the Unavoidable Impacts on page 77, the second  
23 item listed, Natural Resources. It lists consumption of natural  
24 resources and it talks about primarily crushed rock and asphalt for  
25 building materials. In mitigation measures, none. I was wondering if  
26 possibly the consumption of agricultural land should not be listed in  
27 conjunction with that. You have it listed in a previous page, in  
28 the following page, but it seems to me that it should be listed under



1 natural resources also. And mitigation measures to follow.

2 MIKE SHUTT: Okay, that is addressed on the next page on Vegetation.  
3 Probably not, maybe not as clearly as you'd like it. Do you see that,  
4 Jim, on page 78?

5 JIM KELLEY: Yes, I see that

6 MIKE SHUTT: That still isn't adequate?

7 RICHARD MITCHELL: We feel that the land is a natural resource  
8 as much as the sand and gravel and the asphalt and should be treated  
9 as importantly

10 MIKE SHUTT: All right. We have no problem with that. That's  
11 how we've addressed it in the report anyway. This is the summary table.

12 RICHARD MITCHELL: Fine

13 BOB BUCHER: Yes, Howard

14 HOWARD WORTHINGTON: Mr. Bucher, Howard Worthington, again. I would  
15 like to make another remark here and emphasize, the gentleman from  
16 Braden Machinery already brought it up, and I'd like to have that  
17 clarified on our case too and I think it ought to be on the EIR report  
18 that we would like to have it understood that the land that would be  
19 adjacent to the airport property could be utilized for or be access  
20 to the airport runway if there was development along that

21 BOB BUCHER: Okay. Anybody else? Okay, I declare the hearing  
22 closed.

23

24

25 I Jacqueline L. King declare and state as follows: I am the Administrative  
26 Aide to the Imperial County Planning Department and I am also the Assistant  
27 Secretary to the Airport Land Use Commission. In that capacity, I  
28 tape-recorded the hearing of the Airport Land Use Commission on August 8,  
1979.

28

1  
2  
3  
4  
5  
6  
7  
8  
9  
10  
11  
12  
13  
14  
15  
16  
17  
18  
19  
20  
21

I certify and declare under penalty of perjury that the foregoing Transcript of Hearing is a true and correct transcript of that hearing prepared by me and contains a true record of the statements and testimony of all persons conducting and appearing at said hearing.

I certify and declare under penalty of perjury that the foregoing is true and correct to the best of my knowledge and that this declaration is executed at El Centro, California, August 15, 1979.

*Jacqueline L. King*  
\_\_\_\_\_  
JACQUELINE L. KING

STATE OF CALIFORNIA  
COUNTY OF IMPERIAL } SS.  
On August 15, 1979 before me,  
the undersigned, a Notary Public in and for said County and State,  
personally appeared JACQUELINE L. KING

\_\_\_\_\_, known to me  
to be the person whose name is subscribed to the  
within instrument and acknowledged that she executed the  
same.

*James Kelley*  
\_\_\_\_\_



FOR NOTARY SEAL OR STAMP

OFFICIAL SEAL  
JAMES KELLEY  
NOTARY PUBLIC — CALIFORNIA  
PRINCIPAL OFFICE IN  
IMPERIAL COUNTY  
My Commission Expires January 21, 1980



# Affidavit of Publication

— OF —

**PUBLIC NOTICE**  
**NOTICE OF PUBLIC HEARINGS**

Notice is hereby given that public hearings will be held by the Airport Land Use Commission on Wednesday, August 8, 1979, in the Airport conference Room, Airport, Imperial, California to discuss the following matters:

At 7:35 p.m. — To hear a discussion on the proposed **East/West Runway for the Imperial Airport** and for a discussion of the Draft Environmental Impact Report prepared for the project.

At 8:00 p.m. — to determine whether a proposed parcel map as submitted by Louise Huston proposing to divide 291 acres into 2 parcels at 2599 Clark Road, Imperial, is consistent with the policies of the Airport Land Use Commission. A Negative Declaration of Environmental Impact was prepared and filed with the county Clerk.

All interested persons are invited to attend and be heard.

RICHARD D. MITCHELL,  
Secretary, Airport Land Use Commission

445—July 26, 1979

NOTICE OF PUBLIC HEARINGS

« « »

Annette Brown

affirms

being duly sworn, ~~deposes~~ and says: That she is and was at all times mentioned herein over the age of twenty-one years and not interested as a party or otherwise in the above named matter. That she is the publisher of the <sup>IMPERIAL VALLEY WEEKLY</sup> ~~IMPERIAL HOMETOWN REVIEW~~ <sup>and</sup> a newspaper of general circulation as defined by Section 4460 of the Political Code, and that said newspaper is issued weekly at the City of <sup>Imperial</sup> ~~Imperial~~, County of Imperial, State of California. That as publisher, she has charge of all the advertisements published in said newspaper. That the

legal notice

of which the annexed clipping is referred to and made part thereof, is a copy, has been published in each regular and entire issue of said newspaper and not in any supplement thereof for a period of one weeks on the following dates, to-wit:

July 26, 1979

I affirm under penalty of perjury that these statements are true and correct.

*Annette Brown*  
Publisher

~~Subscribed and sworn to before me this~~

Signed: 27th day of July, 1979

PUBLIC NOTICE

CONSTRUCTION OF A CROSSWIND RUNWAY

AT

Imperial County Airport  
Imperial, California

On August 8, 1979, at 7:35 p.m., there will be an open Public Hearing on the following proposed airport development.

Acquire 75 acres of land west of the existing Airport to provide for construction of a new runway; construct a runway 4,500 feet in length and 75 feet in width, construct a taxiway 40 feet wide parallel to the runway, install medium intensity runway and taxiway lights, install a visual approach slope indicator, enclose portions of the Dahlia and Newside Canals as well as portions of the North Central Drain and the Newside Drain, close LaBrucherie Road west of the Airport and construct an extension to Brewer Road to provide access to Austin Road.

Place of Meeting. The hearing will be held at the Airport Terminal Building Conference Room, 1101 Airport Road, Imperial, California, during the regular meeting of the Imperial County Airport Land Use Commission Meeting.

Purpose of Hearing. To consider the economic, social, and environmental effects of the airport location and its consistency with the goals and objectives of such urban planning as has been carried out for this area.

Conduct of the Meeting. Members of the County Planning Dept., Airport Land Use Commission and the County's Airport Consultant will, at the outset, present a summary of their views concerning the proposed project's social, economic, and environmental impact and its consistency with local planning efforts.

Other persons present and desiring to do so will then be afforded the opportunity to present written or oral views (whether in favor of, in opposition to, or by way of proposed revision of, the proposed project).

All oral comments will be recorded.

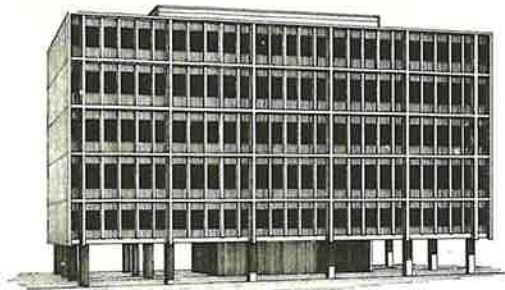
Availability of Environmental Statement. A document summarizing the environmental impact of the airport and the proposed project is available for review during normal working hours at the following locations:

- a. El Centro City Clerk's Office
- b. Airport Manager's Office
- c. County Planning Department
- d. County Clerk's Office
- e. Imperial City Clerk's Office



RICHARD D. MITCHELL, Planning Director  
Airport Land Use Commission, Secretary

# COUNTY OF IMPERIAL



COUNTY SERVICES BUILDING

PLANNING DEPARTMENT  
COURTHOUSE  
EL CENTRO, CALIFORNIA 92243  
(714) 352-8184

Richard D Mitchell  
Planning Director

July 27, 1979

City of Imperial  
201 South Imperial Avenue  
Imperial, California 92251

Gentlemen:

The Imperial County Airport Land Use Commission has received a project application that falls within your area of influence. Attached hereto is a copy of the Notice of Public Hearing.

Our current plans are to hear this during the Airport Land Use Commission meeting of August 8, 1979 at 7:35p.m. Any comments you have should be in writing and must be received by that date.

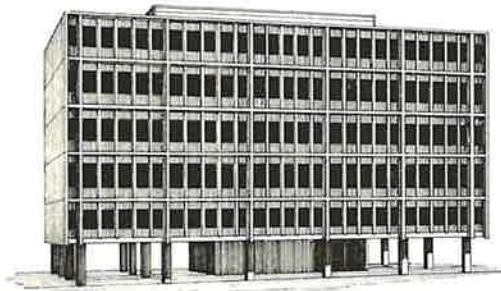
Sincerely yours,

RICHARD D. MITCHELL, Secretary  
Airport Land Use Commission

kb

RE: The proposed East/West Runway and discussion of the EIR prepared for the project

# COUNTY OF IMPERIAL



COUNTY SERVICES BUILDING

PLANNING DEPARTMENT  
COURTHOUSE  
EL CENTRO, CALIFORNIA 92243  
(714) 352-8184

Richard D Mitchell  
Planning Director

July 25, 1979

Airport Land Use Commission  
Courthouse  
El Centro, California  
92243

Gentlemen:

At 7:35 p.m. on August 8, you are to conduct a public hearing to hear public comments concerning the Draft EIR prepared for the Imperial County Airport Master Plan and the East/West Runway. Mr. Mike Shutt with Hodges and Shutt, Incorporated will make a short presentation concerning the Plan and the meeting will be thrown open to public comment and discussion.

A copy of the Environmental Impact Report has been previously provided you for your review. You also will be given the opportunity to question the project and the Environmental Impact Report.

No action is required other than conducting the public meeting.

Sincerely yours,

A handwritten signature in cursive script, appearing to read "Richard D. Mitchell".

RICHARD D. MITCHELL  
Planning Director

RDM/jk

BOARD OF TRUSTEES  
Katie Ellison, President  
Larry Gilbert, Clerk  
Kathy Duggins  
Emma Lou Hansen  
Felix DeRooy

## IMPERIAL UNIFIED SCHOOL DISTRICT

Don W. Hopper  
District Superintendent  
Administration Office  
10th & "E" Streets  
Imperial, Ca. 92251  
Telephone: 355-2433  
Area 714

July 23, 1979

Ben Hulse Elementary School  
Bob Haney, Principal  
355-1185

Westside Elementary School  
Richard Peryam, Principal  
353-0062

Frank Wright Intermediate School  
Joseph D. Kinder, Principal  
355-1334

Imperial High School  
Dr. Fred Heinle, Principal  
James Van Zandt, Vice Principal  
355-1154

Imperial Avenue High School  
John Gary, Teacher  
355-2173

# RECEIVED

JUL 24 1979

IMPERIAL COUNTY  
PLANNING DEPARTMENT

Planning Department  
Courthouse  
El Centro, Calif. 92243

Attn: Mr. Richard D. Mitchell

Dear Mr. Mitchell,

I have examined the Draft Environmental Impact Report on the proposed cross wind runway at the Imperial County Airport. The Imperial School District would not be adversely affected by this project.

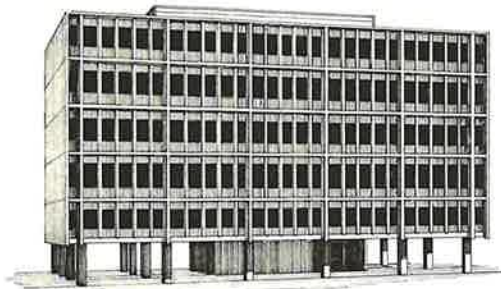
Sincerely,

  
Donald W. Hopper  
District Superintendent

DWH:ejb



# COUNTY OF IMPERIAL



COUNTY SERVICES BUILDING

PLANNING DEPARTMENT  
COURTHOUSE  
EL CENTRO, CALIFORNIA 92243  
(714) 352-8184

Richard D Mitchell  
Planning Director

July 24, 1979

Airport Land Use Commission  
Courthouse  
El Centro, California  
92243

Gentlemen:

At 8:00 on August 8, you are to conduct a public hearing to determine whether Parcel Map #1219 would have a significant effect upon the Imperial County Airport. This particular parcel map provides for the creation of a two acre homesite on Huston Road approximately 4,000 feet east of Clark Road. This particular application is inside the area of influence of the Imperial County Airport and so must come to the Airport Land Use Commission for review.

A Negative Declaration of Environmental Impact was prepared and filed for the project. A copy of the staff report, parcel map and location sketch is attached.

It is recommended that you conduct a public hearing and that you hear all testimony concerning this matter. In view of the fact that this proposed parcel is well outside the clear zone or the noise footprint of any proposed East/West runway, it is recommended that you find that this particular division will have no significant effect upon the Imperial County Airport. This will be reported to the Planning Commission for their consideration of the project.

Sincerely yours,

A handwritten signature in cursive script that reads "Richard D. Mitchell". The signature is written in dark ink and is positioned above the printed name.

RICHARD D. MITCHELL  
Planning Director

Attachments: Staff report  
Parcel map  
Location sketch

RDM/jk

Supervisory District \_\_\_\_\_  
Zoning Map # 5

Hearing Date August 8, 1979  
Agenda Item \_\_\_\_\_

IMPERIAL COUNTY PLANNING DEPARTMENT  
STAFF REPORT

Recommendation:

- \_\_\_\_\_ Approve C/Z from \_\_\_\_\_ to \_\_\_\_\_
  - \_\_\_\_\_ Approve C/P subject to attached conditions
  - \_\_\_\_\_ Deny
  - \_\_\_\_\_ Grant Appeal
  - XXX Approve Parcel Map
- This recommendation is based on the following:

1. Appears to have no impact on Airport
2. \_\_\_\_\_
3. \_\_\_\_\_
4. \_\_\_\_\_

Facts:

Applicant	<u>Louise Huston</u>
Type of Request	<u>Parcel Map</u>
Purpose	<u>Create two parcels</u>
Location	<u>Portion of Tract 99, Township 15 South, Range 14 East</u>
Parcel Size	<u>Two acres</u>
Existing Roads	<u>Huston Road</u>
Existing Land Use	<u>Agriculture</u>
Existing Zoning	<u>A-2</u>
Surrounding Zoning	<u>A-2</u>
General Plan:	Land Use <u>Consistent</u> Noise <u>NA</u>
	Open Space <u>Consistent</u> Scenic Highway <u>NA</u>
	Conservation <u>Consistent</u> Other _____

Plot Plan \_\_\_\_\_

Agency Recommendations: Road/Public Works None  
Health None  
Fire Marshall None  
Other \_\_\_\_\_

Protests: None Received

Environmental Determination: Negative Declaration Filed

Analysis:

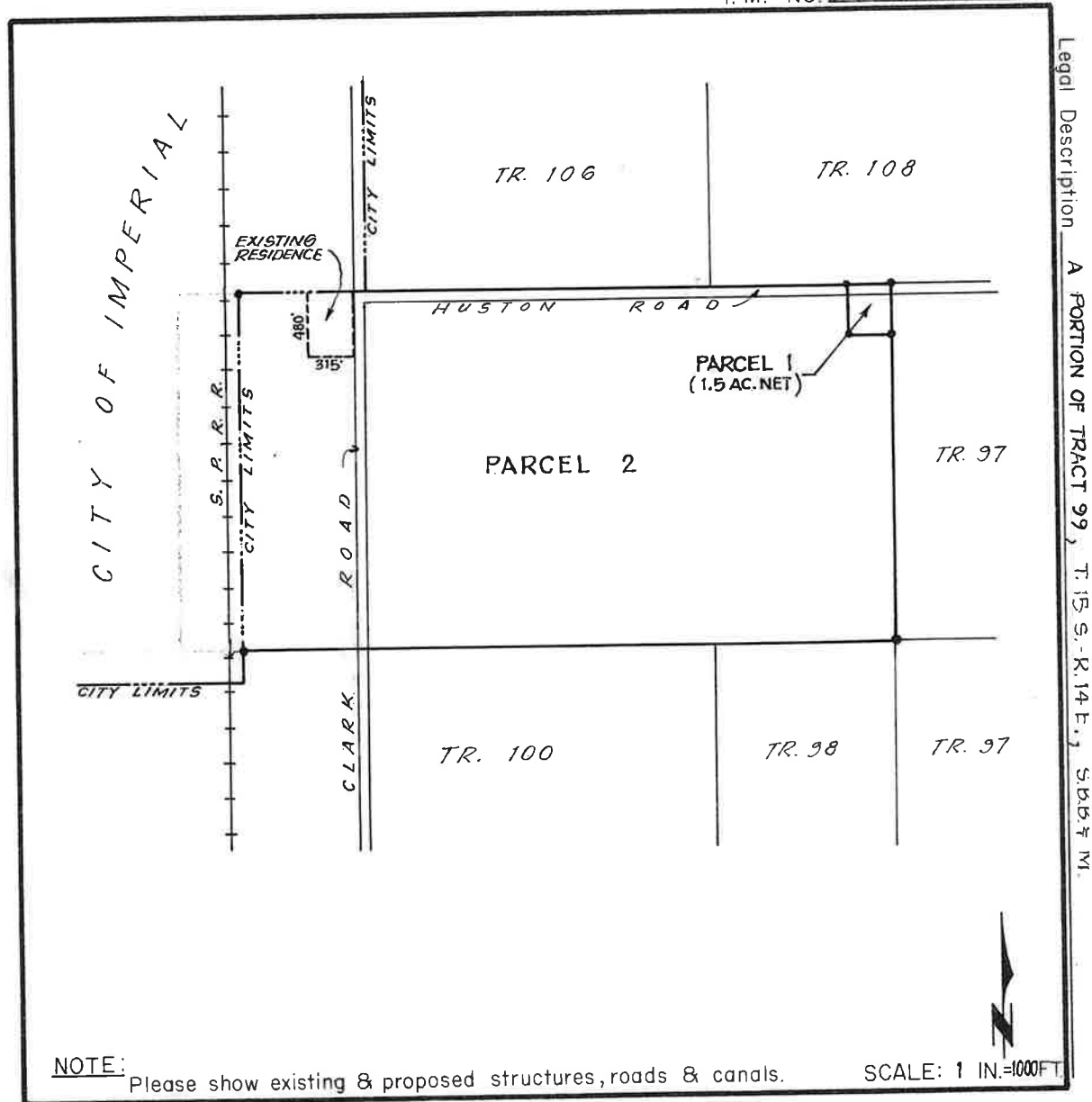
Compatibility and Potential Effect Creates additional homesites  
Consistency with General Plan Consistent  
Environmental Impact Negative  
Other Planning Considerations \_\_\_\_\_

Prepared by: Richard Mitchell/Jim Kelley  
Date: July 24, 1979

# TENTATIVE PARCEL MAP

FILED WITH IMPERIAL COUNTY PLANNING DEPARTMENT

P.M. NO. M-1219



NOTE: Please show existing & proposed structures, roads & canals.

SCALE: 1 IN. = 1000 FT

USE BLACK INK ONLY This form must be reproduced.

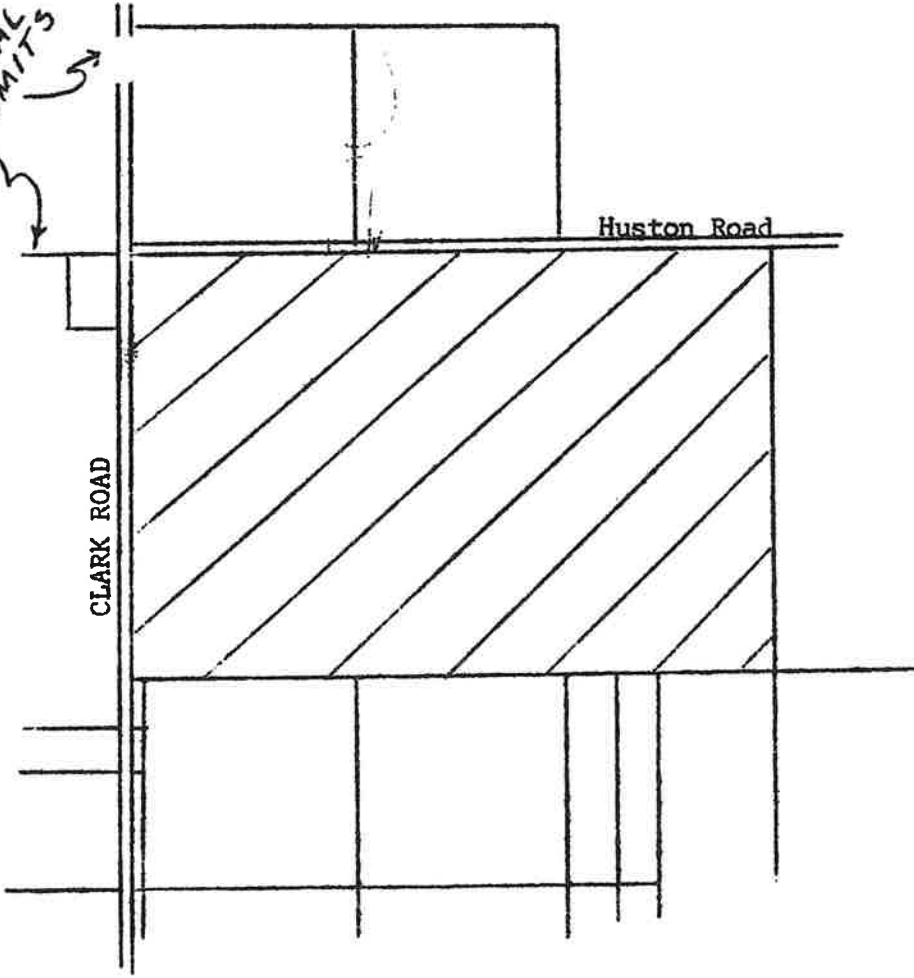
Map prepared by: (~~GWAER~~, Agent) WADDELL ENGINEERING, INC.  
(Print)

Address P. O. BOX 2280 (444 South 8th Street)

City El Centro, CA 92243 Telephone (714) 352-6670

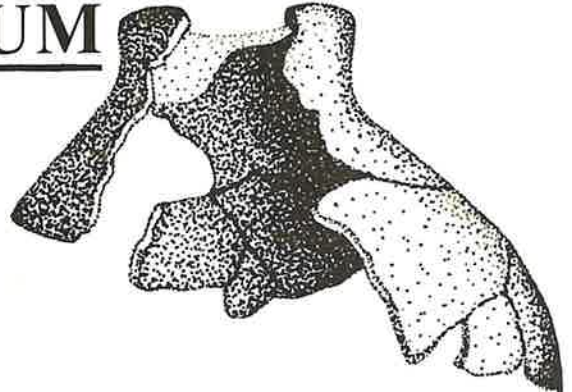
DEPARTMENTAL USE ONLY:	
Zone: <u>A-2</u>	Filing Date: <u>7/3/79</u>
Supervisory District: _____	Review Date: _____
A.P. NO: _____	Re: _____
T.P.M. NO. _____	

IMPERIAL  
CITY LIMITS



1" = 1200'

# IVC MUSEUM



RECEIVED

JUL 13 1979

IMPERIAL COUNTY  
PLANNING DEPARTMENT

July 10, 1979

Richard D. Mitchell, Planning Director  
Imperial County Planning Dept.  
940 W. Main  
El Centro, Ca 92243

Dear Mr. Mitchell:

Re: DRAFT EIR FOR CROSSWIND RUNWAY  
PROJECT: Imperial Co. Airport

The archaeological survey for the above EIR project was negative. As far as we are concerned there would be no archaeological impact for this area.

Sincerely,

A handwritten signature in ink, appearing to read "Lora L. Cline", written over a series of overlapping loops.

Lora L. Cline  
Site Recorder

SERVING IMPERIAL COUNTY



Santa Rosa, California  
Portland, Oregon

HODGES & SHUTT  
AVIATION PLANNING SERVICES



2310 Airport Boulevard • Santa Rosa, California 95401 • (707) 526-5010

June 22, 1979

RECEIVED

JUN 27 1979

IMPERIAL COUNTY  
PLANNING DEPARTMENT

Mr. Dan Pavao  
Airport Manager  
Imperial County Airport  
1101 Airport Road  
Imperial, California 92251

SUBJECT: Draft Environmental Impact Report

Dear Dan:

We have shipped, via Greyhound, 30 copies of the subject report for local circulation. Additionally, we have forwarded 21 copies of the document to the State Clearinghouse to commence the A-95 review process. We have enclosed for your review a suggested notice concerning the environmental hearing that will be required for this project. Certain specifics, such as time and date of the hearing and the availability of local environmental documents, should be prepared by your office. This announcement follows FAA guidelines and will satisfy its requirements for advertising the project.

Please advise us if there is any additional information required concerning this matter.

Best regards,

Michael A. Shutt  
Partner

MAS:cd

cc: ✓ Mr. Richard D. Mitchell

PUBLIC NOTICE

CONSTRUCTION OF A CROSSWIND RUNWAY

at

Imperial County Airport  
Imperial, California

On August 8, 1979, at 7:35 PM there will be an open Public Hearing on the following proposed airport development.

Acquire 75 acres of land west of the existing Airport to provide for construction of a new runway; construct a runway 4,500 feet in length and 75 feet in width, construct a taxiway 40 feet wide parallel to the runway, install medium intensity runway and taxiway lights, install a visual approach slope indicator, enclose portions of the Dahlia and Newside Canals as well as portions of the North Central Drain and the the Newside Drain, close LaBrucherie Road west of the Airport and construct an extension to Brewer Road to provide access to Austin Road.

Place of Meeting. The hearing will be held at the Airport Terminal Building Conference Room, 1101 Airport Road, Imperial, California, during the regular meeting of the Imperial County Airport Land Use Commission Meeting.

Purpose of Hearing. To consider the economic, social, and environmental effects of the airport location and its consistency with the goals and objectives of such urban planning as has been carried out for this area.

Conduct of the Meeting. Members of the County Planning Dept., Airport Land Use Commission and the County's Airport Consultant will at the outset present a summary of their views concerning the proposed project's social, economic, and environmental impact, and its consistency with local planning efforts.

Other persons present and desiring to do so will then be afforded the opportunity to present written or oral views (whether in favor of, in opposition to, or by way of proposed revision of, the proposed project).

All oral comments will be recorded.

Availability of Environmental Statement. A document summarizing the environmental impact of the airport and the proposed project is available for review during normal working hours at the following locations:

- a) El Centro City Clerk's Office
- b) Airport Manager's Office
- c) County Planning Department
- d) County Clerk's Office
- e) Imperial City Clerk's Office

ENVIRONMENTAL IMPACT REPORT CHECKLIST

Project Type Proposed East West Runway  
 Applicant County of Imperial EIR # \_\_\_\_\_  
 Address 1101 Airport Road EA # \_\_\_\_\_  
Imperial, Ca 92251 SCH # \_\_\_\_\_

Notice of Preparation \_\_\_\_\_ Review Period \_\_\_\_\_  
 Draft Mailed (DM) 7-3-79 Review Period 8-13-79  
 Final Mailed (FM) \_\_\_\_\_

COUNTY STAFF

	DM	FM		DM	FM
Board of Supervisors			Public Works David E. Pierson	✓	
Health Department William Hsu	✓		Parks & Recreation Department Richard Pollock		
Imperial County Public Library	✓		Fire Marshall Rudy Medina	✓	
Sheriff's Department Oren Fox	✓		Assessor's Office Walt Lancaster (Geo. only)	✓	
Planning Commission			Agricultural Department Tom Plumb	✓	
Air Pollution Control Harry Dillon	✓		Co. Superintendent of Schools C. Raymond Lowe		

OTHER APPROPRIATE CONTACTS

	DM	FM		DM	FM
1. City of Brawley			2. City of Westmorland		
3. City of Calipatria			4. City of Calexico		
5. City of El Centro	✓		6. City of Holtville		
7. City of Imperial	✓		8. Niland Chamber of Commerce		
9. Heber Pub. Utl. Dist.			10. Imperial Irrigation Dist.	✓	
11. Salton City Comm. Serv.			12. Quechan Tribal Council		
13. Farm Bureau	✓		14. Seeley Co. Water Dist.		
15. Winterhaven Co. Water Dist.			16. Palo Verde Irrigation Dist.		
17. Coachella Valley Co. Wtr.			18. General Telephone		
19. So. Calif. Water Co.			20. So. Calif. Gas Co.	✓	
21. Pacific Telephone Co.	✓		22. Soil Conservation Service		
23. Continental Tel. Co. of Cal.			24. IVC Museum	✓	
25. Calif. Wtr. Quality Control			26. Indian Hill Library		
27. El Centro Public Library	✓		28. Brawley County Library		
29. Govt. Doc. Dept. (Geoth.)			30. CA Resources Agency		
31. State Clearinghouse			32. CA Native Plants Society		
33. I.V. Pioneers Assoc.	✓		34. House of Hospitality (Geoth.)		
35. Brawley Elem. Scho. Dist.			36. Brawley Union H.S. Dist.		
37. Calexico Unified Sch. Dist.			38. Calipatria Unified Sch. Dist.		
39. Heber School Dist.			40. Holtville Unified Sch. Dist.		
41. Magnolia Elem. Sch. Dist.			42. Imperial Unified Sch. Dist.	✓	
43. Mulberry Elem. Sch. Dist.			44. McCabe Unified Sch. Dist.		
45. Seeley Unified Elem. Dist.			46. Westmorland Un. Elem. Sch.		
47. El Centro & Central Sch. Dist.			48. San Pasqual Valley Sch. Dist.		
49. Meadows Union Sch. Dist.			50. Palo Verde Un. Sch. Dist.		
51. Sea View School			52. Valley View School		
53. Brawley Co. Water Dist.					

**EXHIBIT E**  
**(See Next Page)**



1 RESOLUTION DETERMINING CONSTRUCTION  
2 OF T-HANGERS TO BE CONSISTENT WITH  
3 THE AIRPORT LAYOUT PLAN AND THE  
4 IMPERIAL COUNTY GENERAL PLAN

5 WHEREAS, there was submitted to the Imperial County Planning Commission  
6 a request to determine whether construction and leasing of T-hangers on  
7 County property on the west side of First Street inside the airport boundary,  
8 Imperial, is consistent with the General Plan, AND

9 WHEREAS, there was a public hearing held in the Board of Supervisors  
10 Chambers, Courthouse, El Centro, California on January 9, 1980 at 10:35 a.m.,  
11 AND

12 WHEREAS, it was the findings of the Commission that this use is  
13 consistent with the Airport Layout Plan and the Imperial County General Plan,

14 NOW THEREFORE BE IT RESOLVED the construction and leasing of T-hangers  
15 is found to be consistent with the General Plan of Imperial County and  
16 with the Airport Layout Plan.

17 BE IT CERTIFIED that the Negative Declaration was certified to have  
18 been properly prepared and was considered by the Planning Commission in  
19 reaching a decision.

20 Motion made by Commissioner Villalobos, seconded by Commissioner DiMasio  
21 and carried on the affirmative roll call vote of Commissioners Jones,  
22 Mendivil, Johnson, Manchester, DiMasio, Villalobos and Brown.

23 This is to certify that the foregoing is a true and correct copy of  
24 a resolution passed by the Imperial County Planning Commission at a  
25 regular meeting January 9, 1980 in the Board of Supervisors Chambers,  
26 Courthouse, El Centro, California.

27 Don Brown, Chairman

28 

RICHARD D. MITCHELL, Secretary  
Imperial County Planning Commission



# Affidavit of Publication

— OF —

NOTICE OF PUBLIC HEARINGS

**PUBLIC NOTICE**

**NOTICE OF PUBLIC HEARING**

Notice is hereby given that public hearings will be held by the Imperial County Planning Commission on Wednesday, January 9, 1980, in the Board of Supervisors Chambers, Courthouse, El Centro, California, to discuss the following matters:

At 9:05 a.m. — To determine the advisability of granting an appeal from denial of Parcel Map No. 1253 submitted by Fred Sessions. This matter was continued from the hearing of December 12, 1979.

At 9:10 a.m. — To determine the advisability of granting an extension to CUP No. 257-76 issued to Public Works Department. This matter was continued from the hearing of December 12, 1979.

At 9:15 a.m. — To determine the advisability of granting an extension to CUP No. 258-76 issued to W. Sam Estes. This matter was continued from the hearing of December 12, 1979.

At 9:20 a.m. — To hear an appeal from condition requiring paved road as applied to Parcel Map No. 1268 submitted by Paul Benefield for parcels located at 179 Danenberg Road, El Centro. A Negative Declaration of Environmental Impact was prepared and filed with the County Clerk.

At 9:25 a.m. — To hear an appeal from condition requiring paved road as applied to parcel map No. 1269 submitted by Keith Funk requiring paved road at parcels located at 179 Danenberg Road, El Centro. A Negative Declaration of Environmental Impact was prepared and filed with the County Clerk.

At 9:30 a.m. — To determine the advisability of granting a Conditional Use Permit submitted by Pioneers Memorial Hospital in order to establish an infant care center at 207 W. Legion Road, Brawley.

At 9:35 a.m. — To determine whether a zone change from A-1 to A-1-L-2 on parcel located at 2560 Nance Road, Imperial submitted by Hugh Hisel is consistent with the General Plan. A Negative Declaration of Environmental Impact was prepared and

**PUBLIC NOTICE**

Gillett Road, El Centro. A Negative Declaration of Environmental Impact was prepared and filed with the County Clerk.

At 10:05 a.m. — To hear an appeal from minimum lot size for Parcel Map No. 1262 submitted by Arthur Moon for property located at 2185 James Road, El Centro. A Negative Declaration of Environmental Impact was prepared and filed with the County Clerk.

At 10:15 a.m. — The Planning Director has denied the application for Parcel map No. 1263 submitted by Howard Vogt because of potential impact on the high school district, and he is appealing that denial. The project is located at the southeast corner of W. Evan Hewes Hwy. and Higgins Lane. A Negative Declaration of Environmental Impact was prepared and filed with the County Clerk.

At 10:25 a.m. — To determine whether drilling of water wells by the Winterhaven County Water district is consistent with the General Plan. A Negative Declaration of Environmental Impact was prepared and filed with the County Clerk.

At 10:35 a.m. — To hear a capital improvement project and the leasing of County property on the west side of First Street inside the airport boundary. A Negative Declaration of Environmental Impact was prepared and filed with the County Clerk.

At 10:45 a.m. — To reconsider the construction requirements of the subdivision proposed in Parcel map No. 1079 submitted by Walter Smith.

At 10:55 a.m. — to hear an appeal from condition of paving requirement on Parcel Map No. 1265 submitted by Property Consultants for a project located on Lots 13 and 24, Tract 55, Township 16 South, Range 9 East. A Negative Declaration of Environmental Impact was prepared and filed with the County Clerk.

At 11:05 a.m. — to determine whether a zone change from Recreational to M-2

★ ★ ★

Annette Brown

affirms and says: That she is and was at all times mentioned herein over the age of twenty-one years and not interested as a party or otherwise in the above named matter. That she is the publisher of the

IMPERIAL VALLEY WEEKLY

and

IMPERIAL HOMETOWN REVIEW

a newspaper of general circulation as defined by Section 4460 of the Political Code, and that said newspaper is issued weekly in the County of Imperial, State of California. That as publisher, she has charge of all the advertisements published in said newspaper. That the

legal notice

of which the annexed clipping is referred to and made part thereof, a copy, which has been published in each regular and entire issue of said newspaper and not in any supplement thereof for a period of one weeks on the following dates, to-wit:

December 27, 1979

I affirm under penalty of perjury that these statements are true and correct.

Annette Brown  
Publisher

SIGNED:

This 9th day of January, 19 80

# Affidavit of Publication

— OF —

**LEGAL**

**PUBLIC NOTICE  
REGARDING NEGATIVE  
DECLARATIONS**

The Imperial County Environmental Evaluation Committee met on January 3, 1980, to review projects and have directed that a Negative Declaration of Environmental Impact be filed with the County Clerk on the following:

Assessment No. 190-79: Applicant — Arthur Moon, Parcel Map 1262 proposing to divide a 6.8 acre parcel into two parcels — one 2.2 acre parcel and one 4.6 acres at 2185 James Road, El Centro.

Assessment No. 191-79: Applicant — Howard Vogt, Parcel Map 1263 proposing to divide a 2 acre parcel into two one acre parcels at the southeast corner of W. Evan Hewes Highway and Higgins Lane.

**PUBLIC NOTICE**

Assessment No. 192-79: Applicant — Steve Hester, Parcel Map 1264 proposing to divide one acre into two 1/2 acre parcels at 217 E. Gillett Road, El Centro.

Assessment No. 193-79: Applicant — Charles Johnson, Parcel Map 1266 proposing to divide 1 acre into two parcels at Lot 1, Block 2 of the Holtville Orchards Tract.

Assessment No. 194-79: Applicant — Property consultants, Parcel Map 1265 proposing to create four parcels of 19.9 acres each on Lots 13 and 24, Tract 55, Township 16 South, Range 9 East.

Assessment No. 195-79: Applicant — George Swink, Parcel Map 1267 proposing to divide 67 acres on the south half of the northwest 1/4 of Section 4, Township 11 South, Range 14 East, into a 12 acre parcel and a 55 acre parcel.

Assessment No. 196-79: Applicant — Araz Station, a capital improvement project to determine whether drilling of a new well is consistent with the County's General Plan.

Assessment No. 197-79: Applicants — Keith Funk and Paul Benefield, Parcel Maps 1268 and 1269 proposing to create 8 parcels on a portion of Tract 140, Township 16 South, Range 14 East.

Assessment No. 198-79: Applicant — Hugh Hisel, proposing a change of zone from A-1 to A-1-L-2 on a portion of Tract 47 and 48, Township 15 South, Range 13 East.

Assessment No. 199-79: Applicant — County of Imperial proposing to lease approximately 1.95 acres on airport operating property to private enterprise to construct T-hangers.

Assessment No. 200-79: (Mitigated Negative) Applicant — Phillips Petroleum, proposing to drill 6 geothermal exploratory wells east of Brawley.

RICHARD D. MITCHELL,  
Chairman,, Environmental  
EEvaluation Committee  
640—Jan. 3, 1980

**PUBLIC NOTICE REGARDING NEGATIVE  
DECLARATIONS**

★ ★ ★

Annette Brown

affirms and says: That she is and was at all times mentioned herein over the age of twenty-one years and not interested as a party or otherwise in the above named matter. That she is the publisher of the

IMPERIAL VALLEY WEEKLY

and

IMPERIAL HOMETOWN REVIEW

a newspaper of general circulation as defined by Section 4460 of the Political Code, and that said newspaper is issued weekly in the County of Imperial, State of California. That as publisher, she has charge of all the advertisements published in said newspaper. That the

legal notice

of which the annexed clipping is referred to and made part thereof, a copy, which has been published in each regular and entire issue of said newspaper and not in any supplement thereof for a period of one weeks on the following dates, to-wit:

January 3, 1980

I affirm under penalty of perjury that these statements are true and correct.

*Annette Brown*

Publisher

SIGNED:

This 9th day of January, 1980

INITIAL STUDY

FOR

(Environmental Review of Proposed Project)

Date of Application: \_\_\_\_\_ I.S.N.: 199-79

Project Type: Lease of County Property

Project Name: \_\_\_\_\_

Address of Project Site: 1101 Airport Road

Imperial, California

Project Sponsor: County of Imperial

Address: 940 Main Street

El Centro, California

Telephone Number: 352-3610

Current Property Owner of Record: Same as sponsor

Address: \_\_\_\_\_

Telephone Number: \_\_\_\_\_

Initial Study Prepared By:

Name: JIM KELLEY

Signature: *Jim Kelley*

Name: SARI McCLURE

Signature: *Sari McClure*

Name: \_\_\_\_\_

Signature: \_\_\_\_\_

Findings

By  
E.E.C.

EIR

Negative Declaration

RICHARD D. MITCHELL  
Planning Director  
EEC Chairperson

## **1.0 Introduction**

### **1.1 Technical Discussion of Proposed Project**

The County of Imperial acting through the Airport Manager is proposing to lease out approximately 1.95 acres on airport operating property to private enterprise to construct T-hangers on the west side of First Street just inside the airport boundary. The proposal would be to construct hanger space for 18 airplanes. At this point, the terms of the lease are unknown although it is assumed that it will be a long term lease so that the private lessee may recapture his investment.

### **1.2 Project Location**



2.0 Probable Impact of Proposed Project on Environment (All "Yes" and "Maybe" Answers Will Be Discussed In Environmental Impact Report).

Yes      Maybe      No

2.1 Geology, Will the proposal result in:

2.1.1 Landform and topography

a. Change in topography or ground surface relief features?  Yes     Maybe     No

b. Other impacts \_\_\_\_\_

\_\_\_\_\_

2.1.2 Structural Geology

a. Unstable earth conditions or in changes in geologic substructures?  Yes     Maybe     No

b. The destruction, covering or modification of any unique geologic or physical features?  Yes     Maybe     No

c. Other impacts \_\_\_\_\_

\_\_\_\_\_

2.1.3 Geologic Hazards

a. Exposure of people or property to geologic hazards such as earthquakes, landslides, mudslides, ground failure, or similar hazards?  Yes     Maybe     No

b. Other impacts \_\_\_\_\_

\_\_\_\_\_

2.1.4 Soils

a. Disruptions, displacements, compaction or overcrowding of the soil?  Yes     Maybe     No

b. Any increase in wind or water erosion of soils either on or off the site?  Yes     Maybe     No

\_\_\_\_\_



c. Changes in deposition or erosion of beach sands, or changes in siltation, desposition or erosion which may modify the channel of a river or stream or the bed of the ocean or any bay, inlet or lake?

\_\_\_\_\_

\_\_\_\_\_

X

d. Other impacts \_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

2.2 Hydrology. Will the proposal result in:

2.2.1 Groundwater

a. Alteration of the direction or rate of flow of ground waters?

\_\_\_\_\_

\_\_\_\_\_

X

b. Change in the quantity of ground waters, either through direct ad-  
ditions or withdrawals, or through  
interception of an aquifer by cuts  
or excavations?

\_\_\_\_\_

\_\_\_\_\_

X

c. Other impacts \_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

2.2.2 Surface water

a. Changes in currents, or the course or direction of water movements, in either marine or fresh waters?

\_\_\_\_\_

\_\_\_\_\_

X

b. Changes in absorption rates, drainage patterns, or the rate and amount of surface water run-off?

\_\_\_\_\_

\_\_\_\_\_

X

c. Alterations to the course or flow of flood waters?

\_\_\_\_\_

\_\_\_\_\_

X

d. Change in the amount of surface water in any water body?

\_\_\_\_\_

\_\_\_\_\_

X

e. Discharge into surface waters, or in any alteration of surface water quality, including but not limited to temperature, dissolved oxygen or turbidity?

\_\_\_\_\_

\_\_\_\_\_

X

f. Substantial reduction in the amount of water otherwise available for public water supplies?

\_\_\_\_\_ X

g. Exposure of people or property to water related hazards such as flooding or tidal waves?

\_\_\_\_\_ X

h. Other impacts \_\_\_\_\_

\_\_\_\_\_  
\_\_\_\_\_

\_\_\_\_\_

2.3 Climatology. Will the proposal result in:

a. Alteration of air movement, moisture or temperature, or any change in climate, either locally or regionally?

\_\_\_\_\_ X

b. Other impacts \_\_\_\_\_

\_\_\_\_\_  
\_\_\_\_\_

\_\_\_\_\_

2.4 Air Quality. Will the proposal result in:

a. Substantial air emissions or deterioration of ambient air quality?

\_\_\_\_\_ X

b. The creation of objectionable odors?

\_\_\_\_\_ X

c. Other impacts \_\_\_\_\_

\_\_\_\_\_  
\_\_\_\_\_

\_\_\_\_\_

2.5 Noise. Will the proposal result in:

a. Increases in existing noise levels?

\_\_\_\_\_ X \_\_\_\_\_

b. Exposure of people to severe noise levels?

\_\_\_\_\_ X

c. Other impacts \_\_\_\_\_

\_\_\_\_\_  
\_\_\_\_\_

\_\_\_\_\_

2.6 Biology. Will the proposal result in:

2.6.1 Vegetation (Flora)

a. Change in the diversity of species or number of any species of plants, (including trees, shrubs, grass, crops, microflora and aquatic plants)?

\_\_\_\_\_ X

b. Reduction of the numbers of any unique, rare or endangered species of plants?

\_\_\_\_\_ X

c. Introduction of new species of plants into an area, or in a barrier to the normal replenishment of existing species?

\_\_\_\_\_ X

d. Reduction in acreage of any agricultural crop?

\_\_\_\_\_ X

e. Other impacts \_\_\_\_\_

\_\_\_\_\_  
\_\_\_\_\_

\_\_\_\_\_

2.6.2 Wildlife (Fauna)

a. Change in the diversity of species, or numbers of any species of animals (birds, land animals including reptiles, fish and shellfish, benthic organisms, insects or microfauna)?

\_\_\_\_\_ X

b. Reduction of the numbers of any unique, rare or endangered species of animals?

\_\_\_\_\_ X

c. Introduction of new species of animals into an area, or result in a barrier to the migration or movement of animals?

\_\_\_\_\_ X

d. Deterioration to existing fish or wildlife habitat?

\_\_\_\_\_ X

e. Other impacts \_\_\_\_\_

\_\_\_\_\_  
\_\_\_\_\_

\_\_\_\_\_

2.7 Archaeology and Paleontology. Will the proposal result in:

a. An alteration of a significant archaeological or historical site, structure, object or building?

\_\_\_\_\_

b. An alteration or destruction of a significant paleontological site?

\_\_\_\_\_

c. Other impacts \_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

2.8 Land Use. Will the proposal result in?

a. A substantial alteration of the present or planned land use of the area?

\_\_\_\_\_

b. A substantial alteration of the present zoning of the area?

\_\_\_\_\_

c. Generation of substantial additional vehicular movement?

\_\_\_\_\_

d. Effects on existing parking facilities, or demand for new parking?

\_\_\_\_\_

e. Substantial impact upon existing transportation systems?

\_\_\_\_\_

f. Alterations to present patterns of circulation or movement of people and/or goods?

\_\_\_\_\_

g. Alterations to waterborne, rail or air traffic?

\_\_\_\_\_

h. Increase in traffic hazards to motor vehicles, bicyclists or pedestrians?

\_\_\_\_\_

i. An impact upon the quality or quantity of existing recreational opportunities?

\_\_\_\_\_

j. Other impacts \_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

2.9 Socioeconomics Will the proposal result in?

a. Altering the location, distribution, density, or growth rate of the human population of an area?

\_\_\_\_\_ X \_\_\_\_\_

b. Effecting existing housing, or create a demand for additional housing?

\_\_\_\_\_ X \_\_\_\_\_

c. Have an effect upon, or a need for new or altered governmental services in any of the following areas?

1. Fire protection?

\_\_\_\_\_ X \_\_\_\_\_

2. Police protection?

\_\_\_\_\_ X \_\_\_\_\_

3. Schools?

\_\_\_\_\_ X \_\_\_\_\_

4. Parks or other recreational facilities?

\_\_\_\_\_ X \_\_\_\_\_

5. Maintenance of public facilities, including roads?

\_\_\_\_\_ X \_\_\_\_\_

6. Other governmental services?

\_\_\_\_\_ X \_\_\_\_\_

d. An effect upon or a need for new public health services resulting in:

1. Creation of any health hazard or potential health hazard (excluding mental health)?

\_\_\_\_\_ X \_\_\_\_\_

2. Exposure of people to potential health hazards?

\_\_\_\_\_ X \_\_\_\_\_

3. A risk of an explosion or the release of hazardous substances (including, but not limited to, oil, pesticides, chemicals or radiation) in the event of an accident or upset conditions?

\_\_\_\_\_ X \_\_\_\_\_

e. A need for new systems, or substantial alterations to the following utilities?

1. Power or natural gas?

\_\_\_\_\_ X \_\_\_\_\_

2. Communications systems?

\_\_\_\_\_ X \_\_\_\_\_



- Water? \_\_\_\_\_
- 4. Sewer or septic tanks? \_\_\_\_\_
- 5. Storm water drainage? \_\_\_\_\_
- 6. Solid waste and disposal? \_\_\_\_\_

2.10 Visual Resources. Will the proposal result in?

- a. The obstruction of any scenic vista or view open to the public, or will the proposal result in the creation of an aesthetically offensive site open to public view? \_\_\_\_\_
- b. Production of new light or glare? \_\_\_\_\_
- c. Other impacts \_\_\_\_\_

3.0 Special Conditions and Stipulations:

Mandatory Findings of Significance

- a. Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory? \_\_\_\_\_
- b. Does the project have the potential to achieve short-term, to the disadvantage of long-term, environmental goals? (A short-term impact on the environment is one which occurs in a relatively brief, definitive period of time while long-term impacts will endure well into the future) \_\_\_\_\_
- c. Does the project have impacts which are individually limited, but cumulatively considerable? (A project may impact two or more separate resources where the impact of each resource is relatively \_\_\_\_\_

small, but where the effect of the total of those impacts on the environment is significant.)

\_\_\_\_\_ X

d. Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?

\_\_\_\_\_ X

4.0 Unavoidable Impacts of Proposed Project. Will the proposed project result in:

4.1 Resource base

a. Use of substantial amounts of fuel or energy?

\_\_\_\_\_ X

b. Substantial increase in demand upon existing sources of energy, or require the development of new sources of energy?

\_\_\_\_\_ X

c. Increase in the rate of use of any natural resources?

\_\_\_\_\_ X

d. Substantial depletion of any nonrenewable resource?

\_\_\_\_\_ X

e. Other impacts \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

4.2 Growth inducement

a. A substantial growth of population?

\_\_\_\_\_ X

b. A substantial growth of economic base?

\_\_\_\_\_ X

c. A substantial change in social needs?

\_\_\_\_\_ X

d. Other impacts \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

E.E.C. DETERMINATION

(To be completed by the Environmental Evaluation Committee on the basis of the results found in the Initial Study)

Through Review Of The Initial Study, The Environmental Evaluation Committee Has:

\_\_\_\_\_

Found the proposed project **COULD NOT** have a significant effect on the environment, and a **NEGATIVE DECLARATION WILL BE PREPARED.**

\_\_\_\_\_

Found that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because the mitigation measures described on an attached sheet have been added to the project. A **NEGATIVE DECLARATION WILL BE PREPARED.**

\_\_\_\_\_

Found the proposed project **MAY** have a significant effect on the environment, and an **ENVIRONMENTAL IMPACT REPORT** is required.

Date: \_\_\_\_\_

\_\_\_\_\_  
RICHARD D. MITCHELL  
Planning Director  
EEC Chairperson

For: \_\_\_\_\_

\_\_\_\_\_

jk

COUNTY OF IMPERIAL  
ENVIRONMENTAL IMPACT PROCEDURES

NEGATIVE DECLARATION

County of Imperial  
Responsible

Courthouse  
Address

R. D. Mitchell  
Contact

Planning  
Division

El Centro, Imperial 92243  
City, County Zip

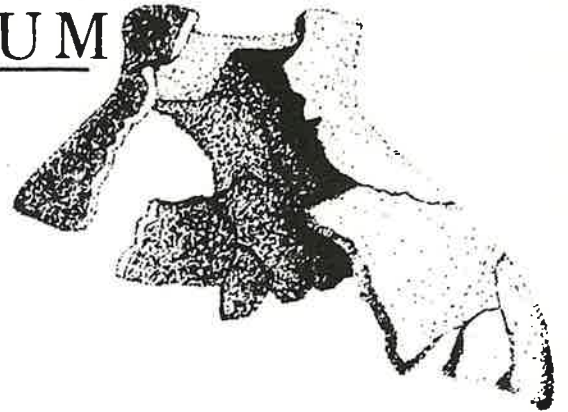
714 352-8184  
Area Code Phone#

Project Title: Lease of County Property Assessment # 199-79

Description of Project:

The County of Imperial acting through the Airport Manager is proposing to lease out approximately 1.95 acres on airport operating property to private enterprise to construct T-hangers on the west side of First Street just inside the airport boundary. The proposal would be to construct hanger space for 18 airplanes. At this point, the terms of the lease are unknown although it is assumed that it will be a long term lease so that the private lessee may recapture his investment.

# I.V.C. MUSEUM



ARCHAEOLOGICAL/PALEONTOLOGICAL REVIEW OF  
IMPERIAL COUNTY ENVIRONMENTAL ASSESSMENT  
Number 199-79

Applicant Name: County of Imperial

Date of Assessment: \_\_\_\_\_

Date Assessment Received: 1-18-80

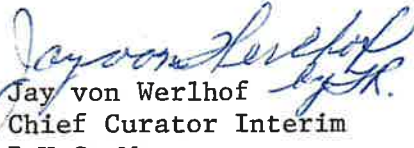
Date of Review: 1-18-80

Reviewed by: Lora L. Cline

*This review is to determine what, if any, archaeological/paleontological resources are present, or possibly present, on or critically near the location of the proposed project, and if the presence or possible presence of such would call for a Draft E.I.R. to be prepared in order to determine what impact the proposed project would or could have on such resources.*

- |  | YES | NO  |
|--|-----|-----|
| 1. Sites recorded within 3 miles from proposed project.              | ( ) | (X) |
| 2. Sites recorded within less than 3 miles from proposed project.    | ( ) | (X) |
| 3. Sites recorded on project location or immediately adjacent to it. | ( ) | (X) |
| 4. Sites suspected to be on project location.                        | ( ) | (X) |
| 5. Recommendations for archaeological study.                         | ( ) | (X) |

Sincerely,

  
Jay von Werlhof  
Chief Curator Interim  
I.V.C. Museum

SERVING IMPERIAL COUNTY



IMPERIAL COUNTY PLANNING DEPARTMENT  
STAFF REPORT

Recommendation:

- Approve C/Z from \_\_\_\_\_ to \_\_\_\_\_
  - Approve CUP subject to attached conditions
  - Deny
  - Grant Appeal
  - Find project with General Plan.
- This recommendation is based on the following:

1. Coincides with adopted Airport Land Use Plan.
2. \_\_\_\_\_
3. \_\_\_\_\_
4. \_\_\_\_\_

Facts:

Applicant County of Imperial  
Type of Request Capital Improvement Project  
Purpose Build 18 "T" Hangers  
Location County Airport  
Parcel Size 1.95 acres  
Existing Roads First Street - Imperial  
Existing Land Use Airport related  
Existing Zoning "K" Zone  
Surrounding Zoning "K" and R-1  
General Plan: Land Use Consistent Noise Consistent  
Open Space N/A Scenic Highway N/A  
Conservation N/A Other \_\_\_\_\_

Plot Plan \_\_\_\_\_

Agency Recommendations: Road/Public Works None  
Health None  
Fire Marshall None  
Other None

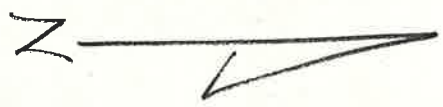
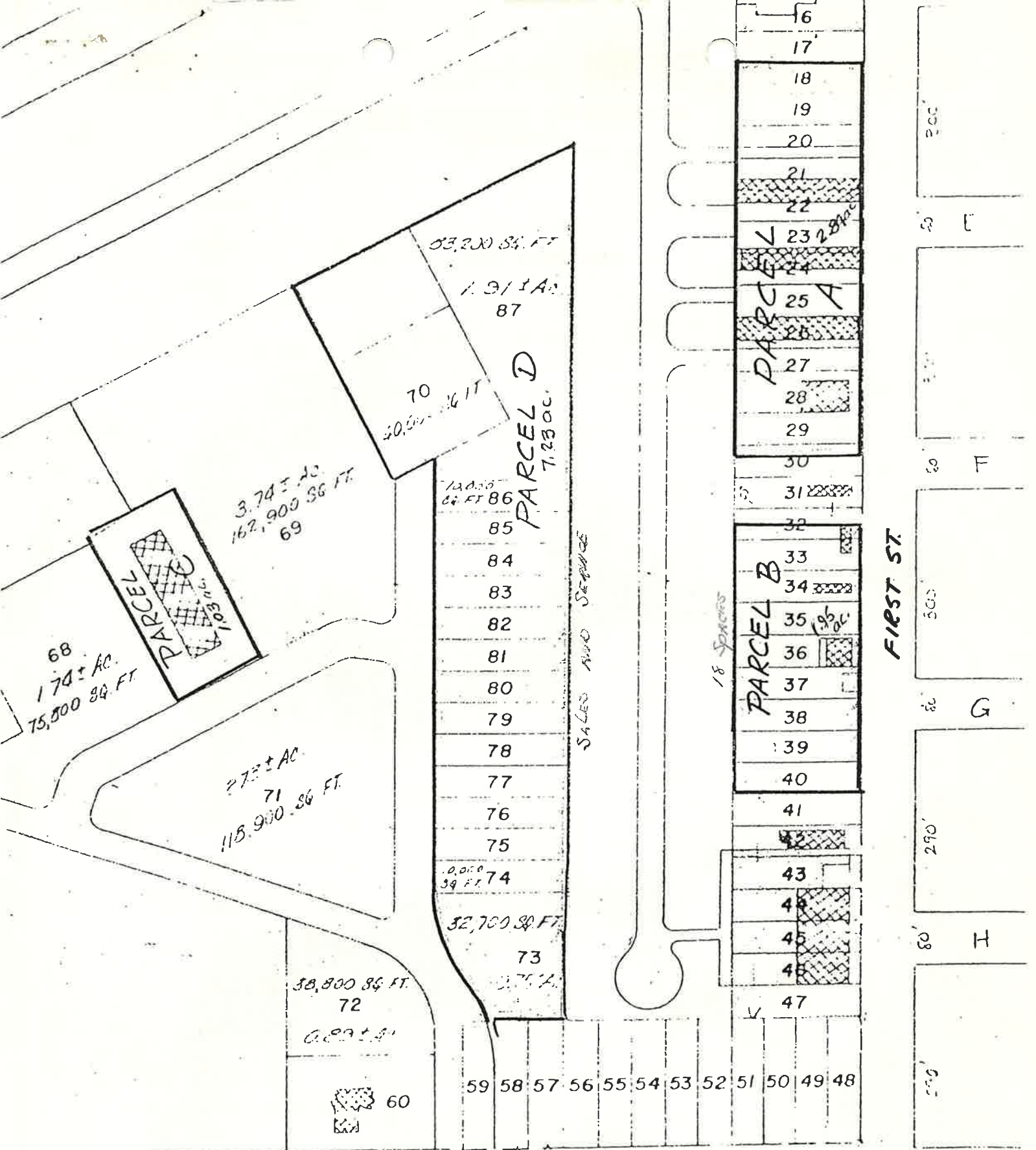
Protests: None received

Environmental Determination: Negative Declaration Filed

Analysis:

Compatibility and Potential Effect Increase hanger availability  
Consistency with General Plan Consistent  
Environmental Impact Negative  
Other Planning Considerations \_\_\_\_\_

Prepared by: Jim Kelley & Richard Mitchell  
Date: 12/31/79



BY  
DATE  
-72  
DRAWN

IMPERIAL COUNTY  
INTER-OFFICE MEMO

DATE December 13, 1979

TO: County of Imperial Planning Department  
FROM: Airport Manager/Chief, Real Property *J. Blawie*  
SUBJECT: APPLICATION ENVIRONMENTAL INFORMATION FORM  
AIRPORT DEVELOPMENT/MANAGEMENT LEASE

RECEIVED

DEC 13 1979

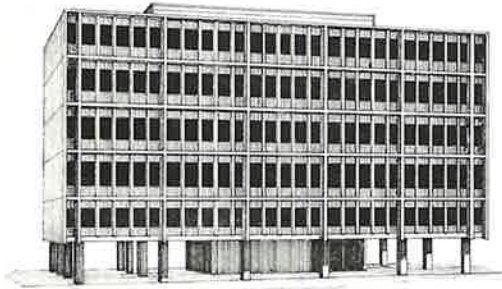
IMPERIAL COUNTY  
PLANNING DEPARTMENT

This office has been in negotiations with local private citizen group for a lease which will provide for construction of aircraft storage hangars, assumption of management of County-owned aircraft storage facilities, and potentially, a fixed-base-operator facility.

Said negotiations are nearing completion.

In this regard, attached is completed Form #201 for processing by your Department

# COUNTY OF IMPERIAL



COUNTY SERVICES BUILDING

PLANNING DEPARTMENT  
COURTHOUSE  
EL CENTRO, CALIFORNIA 92243  
(714) 352-8184

Richard D Mitchell  
Planning Director

January 3, 1980

Imperial County Planning Commission  
Courthouse  
El Centro, CA 92243

Dear Commissioners:

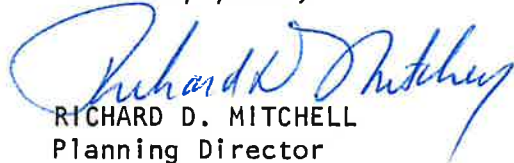
Item #15 on your agenda is a public hearing to determine whether construction of T-hangers and the leasing of county property at the Imperial County Airport is consistent with the Imperial County General Plan. The proposed project is to construct 18 T-hangers on the west side of First Street and to consider leasing these hangers and the balance of the T-hangers for management by a private company.

A Negative Declaration of Environmental Impact was prepared and filed for the project. A copy of the staff report and sketch of the area involved is attached. This project appears to be consistent with the Airport Layout Plan recently approved by the Board of Supervisors.

It is requested that you conduct a public hearing and that you hear all testimony concerning this matter. It is then recommended that you take the following action:

1. Certify that the Negative Declaration was properly prepared and that you considered it in reaching your decision.
2. Find that the project for construction of T-hangers to be consistent with the Airport Layout Plan and the Imperial County General Plan.

Sincerely yours,

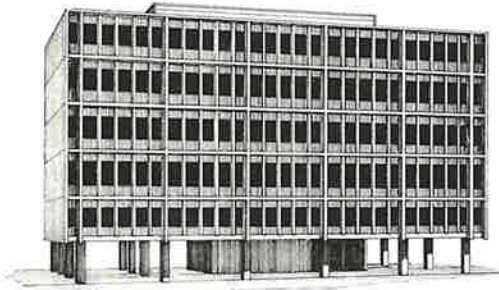


RICHARD D. MITCHELL  
Planning Director

Attachments: Staff report  
Sketch

RDM/jt

# COUNTY OF IMPERIAL



COUNTY SERVICES BUILDING

PLANNING DEPARTMENT  
COURTHOUSE  
EL CENTRO, CALIFORNIA 92243  
(714) 352-8184

Richard D Mitchell  
Planning Director

December 28, 1979

Mr. Dan Pavao  
Imperial County Airport  
1101 Airport Road  
Imperial, CA 92251

Dear Mr. Pavao:

This is to advise you that the Planning Commission has set a public hearing to hear your application for leasing of County property in the Board of Supervisors Chambers, Courthouse, El Centro, California, at their meeting on January 9, 1980 at 10:35 a.m.

It is recommended that you be present to answer questions and make any presentations you may desire. It is necessary that you or a representative be present in order for the Planning Commission to take any action.

Sincerely yours,

RICHARD D. MITCHELL  
Planning Director

rb

Form # 505



APPLICATION ENVIRONMENTAL INFORMATION FORM  
(To be completed and filed by project sponsor)

(Print or Type)

(Attach Additional Sheet As Necessary)

DATE: 12-7-79

PROJECT TYPE:

COMMERCIAL  
DEVELOPMENT

1. Project Name: COUNTY AIRPORT HANGAR DEVELOPMENT / MGMT PROJECT

2. Project Address: 1101 AIRPORT RD., IMPERIAL CA 92251

3. Project Sponsor: IMPERIAL COUNTY AIRPORT DEPT.

Address: 1101 AIRPORT RD., IMPERIAL CA 92251

Phone No.: 355-2894

4. Current Property Owner of Record: COUNTY OF IMPERIAL

Address: COURTHOUSE, EL CENTRO CA 92243

Phone No.: \_\_\_\_\_

5. Legal Description of Project Location: SEE ATTACHED DESCRIPTION  
FROM LEASE.

6. Assessors Block and Lot number: 043-500-15

7. Existing Zone Designation For Project Site: I-2

8. County Permit Applications (Permit type and register number): \_\_\_\_\_

9. List name and address of all jurisdiction in who's boundaries the project site lies. List all permits required by each agency, (school district; city; region; state; federal; etc.): COUNTY OF IMPERIAL,

CITY OF IMPERIAL, IMPERIAL UNIFIED SCHOOL DIST.

10. Project's Proposed Use: <sup>(a)</sup> CONSTRUCTION OF AIRCRAFT STORAGE FACILITY (T-HANGARS). <sup>PARCEL B</sup> (b) ASSUMPTION OF MANAGEMENT OF EXISTING COUNTY-OWNED T-HANGARS & OPEN SHADE HANGAR <sup>PARCELS A & C;</sup> (c) FUTURE HANGAR/FIXED BASE OPERATOR DEVELOPMENT (PARCEL D).

11. Technical Discussion of Proposed Project. (Designs, maps, engineering reports, costs, technical studies, etc.) (Attach additional material as necessary): PROJECT CONSTRUCTION TO CONSIST OF CONCRETE SLABS & FOUNDATIONS; ALL-STEEL T-HANGAR BUILDINGS; ALL NECESSARY ASPHALT TAXIWAY PAVING BETWEEN HANGARS AND CONNECTING TO EXISTING EAST-WEST AIRCRAFT MOVEMENT AREA (TAXIWAY). TWO OF PARCELS (A & C) ARE FULLY DEVELOPED.

12. Site Size: 4 PARCELS (13<sup>+</sup> Acres) see attachment.

13. Number of Floors 1 Ground Floor Sq. Ft. VARIOUS

Total Sq. Ft. \_\_\_\_\_

14. Number of off-street parking spaces: \_\_\_\_\_

15. Project Scheduling: PARCEL B: immediate / PARCEL D: 18 MOS. <sup>+</sup>

16. Incremental Development Yes  No \_\_\_\_\_

First Stage PARC. B. Second Stage PARC. D. Other \_\_\_\_\_

17. Residential Development: Yes \_\_\_\_\_ No

If yes, provide:

a) Number of units (total) \_\_\_\_\_

b) Schedule of Estimated Sale Prices or Rent Range: (Attach)

c) Schedule of Unit Sq. Ft.: (Attach)

d) Estimated Average Household Size: \_\_\_\_\_  
(Number of persons per unit)

18. Commercial Development: Yes  No \_\_\_\_\_

If yes, provide:

a) Type METAL HANGARS FOR AIRCRAFT STORAGE (PUBLIC)

b) Orientation: Neighborhood \_\_\_\_\_; Central City \_\_\_\_\_

Regional

c) Sales Area Sq. Ft. \_\_\_\_\_

Storage Area Sq. Ft. \_\_\_\_\_

d) Loading Facilities \_\_\_\_\_

19. Industrial Development: Yes \_\_\_\_\_ No X

If yes, provide:

a) Type \_\_\_\_\_

b) Employment: Per Shift \_\_\_\_\_ Total \_\_\_\_\_

c) Loading Facilities: \_\_\_\_\_

20. Institutional Development: Yes \_\_\_\_\_ No X

if yes, provide:

a) Major Function: \_\_\_\_\_

b) Community Benefits: \_\_\_\_\_

c) Employment: Per Shift \_\_\_\_\_ Total \_\_\_\_\_

d) Total Occupancy \_\_\_\_\_

e) Loading Facilities: \_\_\_\_\_

21. If the project involves an application for a Variance, Conditional Use Permit, or Rezoning; state clearly those reasons, or describe existing conditions which make these applications necessary: NO.

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

22. Will any of the following items be effected by the approval and subsequent development of the proposed project? On a separate attached sheet or sheets of paper discuss the effects of each item marked "Yes". Also discuss measures taken to eliminate or minimize those effects.

	Yes	No
a) Change in existing features of any:		
1. Beaches	_____	_____X
2. Lakes	_____	_____X
3. Hills	_____	_____X
4. Significant Ground Contours	_____	_____X
b) Change in Scenic Views or Vistas from:		
1. Existing residential areas	_____	_____X
2. Public Lands	_____	_____X
3. Roads	_____	_____X
c) Change in the project's general area's existing development:		
1. Patterns	_____	_____X
2. Scale	_____	_____X
3. Character	_____	_____X
d) Will significant amounts of solid waste or litter be produced by this project?	_____	_____X
e) Will the project have an effect or change the vicinity's:		
1. Dust	_____	_____X
2. Ash	_____	_____X
3. Smoke	_____	_____X
4. Fumes	_____	_____X
5. Odors	_____	_____X

	Yes	No
f) Will this project cause a change in water quality, quantity, or alter existing:		
1. Lakes	_____	_____X
2. Streams	_____	_____X
3. Groundwaters	_____	_____X
4. Surface Drainage Patterns	A _____	_____X
g) Will the project cause a change in existing noise or vibration levels of the general vicinity?	_____X	_____ <i>see attachment</i>
h) Is the proposed project sited on?		
1. Filled land	_____	_____X
2. Slope of 10%	_____	_____X
i) Does the project propose to use or dispose of potentially hazardous material such as:		
1. Toxic substances	_____	_____X
2. Flammables	✓ _____X	_____ <del>X</del> <i>see attachment</i>
3. Explosives	_____	_____X
4. Other _____	_____	_____X
j) Will the project cause a change of demand for public services such as:		
1. Police	_____	_____X
2. Fire Control	_____	_____X
3. Water Supply	_____	_____X
4. Sewage	_____	_____X
5. Street and road maintenance	_____	_____X
6. Education facilities	_____	_____X
7. Health	_____	_____X
8. Other _____	_____	_____



Yes

No

- k) Will the project substantially increase the consumption of energy resources?  Yes  No
- l) Is this project a part of portion of an overall larger project or projects?  Yes  No

23. Describe the project site as it exists before the project, including information on topography, soil stability, plants and animals, and any cultural, historical, or scenic aspects. Describe any existing structures on the site, and use of the structures. Attach photographs of the site. Snapshots or polaroid photos will be accepted. *see attachment*

24. Describe the surrounding properties, including information on plants and animals and any cultural, historical, or scenic aspects. Indicate the type of land use (residential, commercial, etc.), intensity of land use (one-family, apartment houses, shops, department stores, etc.). Attach photographs of the vicinity. Snapshots or polaroid photos will be accepted. *see attachment.*

**CERTIFICATION:** I hereby certify that the statements furnished above and in the attached exhibits present the data and information required for the initial evaluation to the best of my ability, and that the facts, statements, and information presented are true and correct to the best of my knowledge and belief.

Date: 12-7-79

*Daniel B. Pavao*  
 Signature *Airport Mgt / Ch. Real Property.*

For *County of Imperial.*

22. (i) (2) Flamables: Parcel D envisions fixed base

operator development (underground fuel storage). This potential hazard is mitigated by the existence of the new CFR facility on the west side of the airport.

23. Project site is described as follows:

Parcel A: Contains five 6-units metal T-hangars presently managed by County. Hangars have concrete floors with asphalt paving between hangars and taxiway connections. No additional development possible.

Parcel B: Contains two Quonset huts, a wood-frame metal shop building and steel open shade, all formerly used by County Fire Department. Quonset huts and metal shop building are in deteriorating condition. Quonset huts are used for Fire Dept. storage (emergency, hospital equipment etc.). Lessee will be responsible for removal of Quonset and shop building. Fire Dept. will dismantle open shade and relocate it to new CFR facility on west side of airport.

Parcel C: Contains one 10-unit open shade metal hangar with concrete floor, and asphalt paving connecting to airport taxiway. No additional development possible.

Parcel D: Contains 7.23 acres undeveloped land. Projected development estimated to begin 18 months after Parcel B. Projected use will be fixed base operator, or additional hangar storage, as required.

The site is extremely flat, with maximum slope to the north. There are no plants, animals, cultural, historical or scenic aspects of any significance.

The site has been within the airport boundary at least 55 years.

24. Surrounding Properties.

The site is bounded on the north by City of Imperial's First Street with single-family housing throughout. East of Parcel B is VISCO FLYING CO., an aerial applicator firm.

The property to the south to Aten Road (1 mile) and west to LaBrucherie Road (.5 mile) is within the airport boundary.

The property within the airport boundary (exclusive of runway, taxiway and clear-zone limitations) has potential for commercial development.

22,(g)

Aircraft start-up and taxiing is done at low power settings, producing minimal additional noise. While an increase in ambient noise level can be expected from uses associated with this type of aircraft activity, it is not anticipated that this noise will be severe enough to constitute an impact on the residential area to the north.

NOTICE OF PUBLIC HEARING

FOR A

LEASE OF COUNTY PROPERTY

Notice is hereby given that a public hearing to determine whether the County of Imperial should lease to private enterprise will be heard Wednesday, January 9, 1980 at 10:35 a.m., in the Board of Supervisors Chambers, Courthouse, El Centro, California, by the Imperial County Planning Commission.

The proposal was submitted through the Airport Manager in order to lease approximately 1.95 acres on airport operating property for construction of T-hangers on the west side of First Street just inside the airport boundary.

A Negative Declaration of Environmental Impact was prepared and filed with the County Clerk.

All interested persons are invited to attend and be heard.

  
RICHARD D. MITCHELL, Secretary  
Imperial County Planning Commission

jt

NOTICE OF PUBLIC HEARING  
FOR A  
LEASE OF COUNTY PROPERTY

Notice is hereby given that a public hearing to determine whether the County of Imperial should lease to private enterprise will be heard Wednesday, January 9, 1980 at 10:35 a.m., in the Board of Supervisors Chambers, Courthouse, El Centro, California, by the Imperial County Planning Commission.

The proposal was submitted through the Airport Manager in order to lease approximately 1.95 acres on airport operating property for construction of T-hangers on the west side of First Street just inside the airport boundary.

A Negative Declaration of Environmental Impact was prepared and filed with the County Clerk.

All interested persons are invited to attend and be heard.

  
RICHARD D. MITCHELL, Secretary  
Imperial County Planning Commission

jt



NOTICE OF PUBLIC HEARING  
FOR A  
LEASE OF COUNTY PROPERTY

Notice is hereby given that a public hearing to determine whether the County of Imperial should lease to private enterprise will be heard Wednesday, January 9, 1980 at 10:35 a.m., in the Board of Supervisors Chambers, Courthouse, El Centro, California, by the Imperial County Planning Commission.

The proposal was submitted through the Airport Manager in order to lease approximately 1.95 acres on airport operating property for construction of T-hangers on the west side of First Street just inside the airport boundary.

A Negative Declaration of Environmental Impact was prepared and filed with the County Clerk.

All interested persons are invited to attend and be heard.

  
RICHARD D. MITCHELL, Secretary  
Imperial County Planning Commission

jt

Mr. Dan Pavao-  
Imperial County Airport  
1101 Airport Road  
Imperial, CA 92251

City of Imperial  
201 South Imperial Ave.  
Imperial, CA 92251